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The Voice of SSPC: The Society for Protective Coatings
Cover photo: ©iStockphoto.com/antikainen

FEATURES

26 **COATING INDUSTRY'S BEST HONORED AT SSPC 2016**

***By Amy Woodall*, PaintSquare News**

This article recaps SSPC's Annual Business Meeting and Awards Luncheon, held Jan. 18 at SSPC 2016 featuring GreenCOAT in San Antonio. Awards presented included the tenth annual SSPC Structure Awards, given for outstanding coatings projects completed on a variety of interesting and complex structures, as well as a number of individual awards for various coating industry leaders.

32 **MOISTURE TESTING IN BELOW-GRADE CONCRETE WASTEWATER STRUCTURES**

By R.A. Nixon, Corrosion Probe, Inc./Corrosion Testing Laboratories, Inc.

The author discusses methods currently used for testing moisture in below-grade concrete structures in wastewater applications. The article cites real-world examples that demonstrate the shortcomings in various situations and makes practical recommendations for determining whether or to coat.

51 **2016 ANNUAL DIRECTORY OF INDUSTRIAL PAINTING CONTRACTORS**

The *JPCL* Annual Directory of Industrial Painting Contractors includes information about more than 1,600 painting contractor companies primarily located in North America, but also from around the world. Listings are displayed in alphabetical order by country, and then by state or province. The information included was obtained through a survey of painting contractors known to *JPCL*.

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Using SSPC-Guide 20 for Successful Floor Coating Application

SSPC Technology Guide 20, Guide for Applying Thick Film Coatings and Surfacings Over Concrete Floors, provides basic information about the application of thick-film resinous coating systems over concrete floors. These coating thicknesses are defined as being greater than 500 micrometers (µm) (20 mils).

Rather than being a step-by-step instruction, the Guide is intended to close some gaps that may impact both specifiers and contractors when specifying and performing installation projects. It's organized in the same sequence as an actual flooring installation project. It begins by discussing some basic plan considerations for the concrete substrate itself, giving a brief background on concrete mixtures and proper design of the concrete surface over which the floor is to be installed, because the performance of the floor depends on the quality of the substrate. The next section explains the importance of conducting a condition assessment of the existing concrete prior to submitting a bid. A condition assessment can be an evaluation of a newly installed concrete substrate or a survey of an existing substrate that will identify repairs or decontamination procedures that must be completed prior to installation of the flooring system. The assessment may also reveal unacceptable amounts of moisture in the concrete slab, which is a common cause of failure of an installed floor. In addition to noting areas of contamination or damage, the presence and types of cracks and joints and the moisture content of the substrate, a condition assessment should also record environmental conditions, such as temperature and humidity fluctuations, which could affect the installation process. Knowing the results of a condition assessment will help the contractor submit a reasonable bid and the specifier or project manager avoid "sticker shock." Both parties can use the results of a condition assessment as discussion points to identify and address potential problems and determine who is responsible for resolving them. The Guide provides a list of items that should be included in a condition assessment.

Because the presence of moisture in a concrete slab is vital to the success of a flooring system applied over it, the Guide describes techniques for performing moisture evaluation of the concrete substrate and explains the information uncovered by various types of tests. It also discusses problems that are specific

SSPC-Guide 20 is just one tool that can assist in successful completion of a floor coating project, such as the coating of floors at the Lay Dam power plant in Alabama, which is one of the 2016 SSPC Structure Award winners. For more on this and other Structure Award projects, see p. 26.
Photo courtesy of Vulcan Painters, Inc.

to concrete substrates, such as the presence of alkali-silica reaction contaminants and proper treatment of other concrete surface defects that must be corrected prior to installation of the flooring system. Various approaches to the treatment of cracks and joints in the floor are discussed. The contractor must decide how cracks and joints in the substrate will be managed to provide the best substrate possible for the installed floor. Preparation for, and installation of, coved bases are briefly discussed and references for more detailed information and instructions are provided. Commonly used repair procedures for surface defects such as bugholes, spalls and exposed rebar are also described.

Once the contractor has determined that the concrete substrate is acceptable for installation of the flooring system, a jobsite plan (layout) should be developed that includes the identification of areas for staging and storage of equipment and materials, the establishment of traffic control areas around the installation and a plan to ensure that the work area has proper lighting and adequate ventilation and temperature control. Areas that require masking off should also be identified and protected.

After the job layout plan is complete, the installation process can begin. Although the coating manufacturer's product data sheets often provide installation guidance, they may not discuss equipment and techniques necessary to properly mix the coating components. Proper techniques and equipment for mixing

components prior to installation are described in the Guide. To avoid entraining air into the mixture, the use of low-speed mixing equipment is recommended. Proper techniques for applying primers are also discussed, including the use of spiked shoes when walking across a primed area.

The Guide describes seven generic flooring systems, including thick-film systems, self-leveling systems, slurry systems, troweled mortar systems, broadcast systems, mat or membrane systems and spray-applied systems. Each description includes one or more considerations that can affect the proper installation or performance of the flooring. For example, failure to properly detect and remove amine blush can create adhesion problems for coatings applied over an epoxy primer. Therefore, the Guide discusses how amine blush is formed, how it can be detected and how to remove it. Troweled mortar systems are best applied in low lighting conditions with task lighting directed across the floor at a low angle to help identify areas where the mortar is out of plane. Mat or fiber-reinforced systems are best rolled from the center of the fabric toward the edges, and then deaerated.

The Guide provides lists of inspection, testing and documentation hold points that should be observed prior to, during and after the application process. Observing these hold points will help identify possible problems so that they can be corrected in-process rather than after-the-fact. Documentation at these points also provides the contractor with evidence that quality control measures are being taken during the installation.

The Guide has two appendices: one that describes some specialty application equipment only encountered when working on concrete surfaces, and the other that provides a checklist to aid contractors in developing their work plans for specific projects.

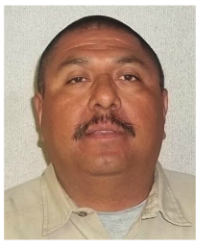
For further information, contact Aimee Beggs, SSPC Standards Development Specialist, who has been with SSPC for 36 years and has managed the SSPC standards development process since 1993. She can be reached at beggs@sspc.org or 412-281-2331, ext. 2223.

SSPC Certifies New Protective Coatings Specialists

A number of coating professionals recently completed certification to become SSPC Protective Coatings Specialists (PCSs).

The PCS certification, SSPC's highest level of certification, recognizes industrial coating professionals for their extensive knowledge in the principles and practices specific to industrial coatings technology. Each individual has been evaluated for his or her mastery of coating type, surface preparation, coating application and inspection, contract planning and management, development of specifications and the economics of protective coatings. SSPC congratulates these individuals for their achievements.

The recently certified Protective Coatings Specialists are:



Joseph Aguilar
KM Industrial
(Long Beach, Calif.)

Dinesh Bankar
Coating Inspection Services India
(Pune, India)

Ji-Hun Cho
PPG SCC Co, Ltd.
(Tongyeong, South Korea)

Andrew Johnson
Corrpro
(Long Beach, Calif.)

Robert Lanterman
KTA-Tator, Inc.
(Pittsburgh, Pa.)

Allan Olano
Jotun Abu Dhabi
(Abu Dhabi, U.A.E.)

Ahren Olson
Covestro LLC
(Pittsburgh, Pa.)

Sonya Peters
Job Box Solutions LLC
(Cape Canaveral, Fla.)

Charles Einer Garcia Rosas
Kaiser
(Santa Cruz, Bolivia)

Webinar Studies Thermal Spray in Marine Settings

SSPC credits are available for participants in a new, free webinar that provides marine professionals with information on thermal spray coatings.

"Thermal Spray Solutions for the Marine Industry," will be presented by Dean Hooks, VP of marketing and sales, Thermion, Inc., on Wednesday, May 18 from 11:00 a.m. to noon, EDT. This webinar is sponsored by Thermion, Inc. and is part of the ongoing 2016 SSPC/*JPCL* Webinar Education Series.

Thermal spray coatings (TSCs) are used extensively for the corrosion protection of steel and iron in a wide range of environments. This webinar will discuss the effectiveness of arc-sprayed aluminum and zinc coatings over long periods of time in a variety of hostile marine environments. TSCs can provide corrosion protection for more than 20 years to first maintenance for marine environments.

Registration, CEU Credits

This program is part of the SSPC/*JPCL* Webinar Education Series, which provides continuing education for SSPC re-certifications and technology updates on important topics. SSPC is an accredited training provider for the Florida Board of Professional Engineers (FBPE), and Professional Engineers in Florida may submit SSPC Webinar Continuing Education Units to the board. To do so, applicants must download the FBPE CEU form and pass the webinar exam, which costs $25. Register for this online presentation at www.paintsquare.com/webinars.

Tnemec Names Bean President, COO

One of the leading manufacturers of high-performance protective coatings has named its new president and chief operating officer.

Kansas City, Mo.-based Tnemec Company Inc. selected Chase Bean to take on the role of president and COO, the company announced in a statement issued March 15. Bean, who brings more than 20 years of experience in the paint and coatings industry to his new position, succeeds Pete Cortelyou, who served as company president for 21 years. In his new role, Bean will assume responsibility over all of Tnemec's operations.

"I look forward to continuing Tnemec's legacy of nurturing the close relationships we have built with our clients," said Bean. "With the help of our operations team, our corporate employees and our representatives in the field, I intend to work on moving our company along the path that Pete [Cortelyou] has paved before me and I am excited about the future for Tnemec," he added.

Cortelyou will continue in his role as chief executive officer until the end of 2016 and will remain the chairman of the board.

Having served as the company's executive vice president for the past eight years, Bean was in charge of managing the sales, marketing, research and development, and technical services departments. Prior to his role as executive vice president, Bean acted as Tnemec's vice president of

sales, director of national accounts, and spent time in the field as a principal sales representative in Minnesota. Earlier in his career, he also worked as both a technical service and customer service representative.

Bean's career with the company included the oversight of product development and commercialization, national account management, and employee development and mentoring — experiences that made him an integral part of the coating manufacturer's growth in the international market, the company said. Bean is also the son of Albert C. "Ace" Bean Jr., the son and namesake of Tnemec's founder.

"While serving Tnemec, Bean has demonstrated great leadership and management skills," Cortelyou said. "Being a fourth-generation coatings expert, he carries with him the same family values and principles that Tnemec holds dear and will help keep the company's traditions steadfast for years to come."

Bean attended The University of the South in Sewanee, Tenn., where he graduated with a Bachelor's degree in English. He also attended the University of Missouri - Kansas City, where he obtained an Executive Master of Business Administration (EMBA) degree.

PPG Rebranding Reflects Evolution

Global paints and coatings manufacturer PPG unveiled an update to its corporate brand, which reflects the transformation that has occurred in the company's identity and portfolio since its inception in 1883.

The new brand campaign, which officially launched March 17, introduces revisions to the corporate logo and tagline and more, while incorporating new uses of the corporate logo on product packaging. In all, the refresh seeks to drive familiarity with the company and better communicate the relationship between the corporate and product brands to the breadth of the company's marketplace. "Over the last decade, PPG has significantly changed its portfolio of products and services and grown its global footprint, and our brand is continuously evolving to reflect who we are today and where we are headed for the future," said Bryan Iams, vice president, corporate and government affairs. "We will be making the PPG brand much more visible, helping to strengthen our brand and our engagement with customers."

One of the changes introduced by the initiative is a new tagline — "PPG: We protect and beautify the world" — which ties together the range of the company's products and services. The updated brand identity also includes a vibrant color palette and more emotive and human imagery, Iams said, which were developed to work together to deliver a more personal and conversational tone.

The public will begin to see more of the new brand through advertisements in trade media and social media. The initial ads will concentrate on the company's range of paint and coatings offerings. As the campaign progresses, ads will showcase specific paint and coatings technologies, including a coating for cars that helps to resist scratches, and a paint for airplanes that uses chemistry found in the skin of an eggplant to reduce heat in the cabin and, in turn, decrease energy use.

Headquartered in Pittsburgh, PPG operates in more than 70 countries with a team of approximately 46,600 employees. The company reported net sales of $15.3 billion in 2015.

Three Certified as SSPC Master Coatings Inspectors

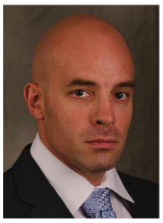
Three veteran protective coatings professionals recently earned SSPC's prestigious Master Coatings Inspector (MCI) status. The goal of the Master Coatings Inspector program is solely to recognize and honor those individuals whose experience and training has afforded them the prestige of multiple inspector certifications. To reach the MCI level, one must qualify for certification as a Concrete Coating Inspector (CCI) as well as two of the four other SSPC certification programs: Bridge Coatings Inspector (BCI); Protective Coatings Inspector (PCI); NAVSEA Basic Paint Inspector course (NBPI), which SSPC administers on behalf of Naval Sea Systems Command; and the Protective Coatings Specialist (PCS) program.

The newly certified MCIs are:

- Gabriel Gonzalez, senior QA inspector, Vigor Industrial (Portland, Ore.);
- Kyle Hough, vice president, Champion Painting (Ft. Lauderdale, Fla.); and
- Gregory Sprinkle, Industrial Inspection & Consulting (Phoenix, Ariz.).

Gonzalez has over 25 years of experience in the protective coatings industry and currently specializes in failure analysis and continuing education for Vigor's employees. He frequently travels to shipyards throughout the U.S., as well as internationally, to assist those in need of his expertise with audits and training. To earn his MCI status, Gonzalez completed the CCI, PCI and NBPI certification programs.

Hough has been involved in the coatings industry for 18 years since joining the IUPAT District Council #21 in the late 1990s. After working for years as an industrial painter in the northeastern and southeastern U.S., he signed on as a partner with Champion Painting in 2011, where he and his brother, Duane (also an SSPC-certified MCI), now run the company's operations. Hough has completed the CCI, PCI and PCS certifications.



Sprinkle also has over 25 years of experience in the industry and currently operates Industrial Inspection & Consulting, an independent firm in Phoenix, Ariz. He specializes in water tanks and concrete coatings and has been active in the local SSPC chapter and committees throughout his career. Sprinkle completed the CCI, PCI and BCI certifications.

For more information on the MCI program, visit www.sspc.org.

THE Buzz on PaintSquare.com

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Now Buzzing on *PaintSquare News*...

Team Lauds 'Swiss Army Knife' of Coatings *(March 1)*

Scientists at a Swiss university have developed a polymer coating they say is so versatile it can be compared to the iconic Swiss army knife, with future applications cutting into the wastewater and shipping industries.

The new polymer for coating materials was designed by a research team at ETH Zurich and spinoff partner Susos in order to prevent biofilms from forming on surfaces. As a result of the technological platform the researchers developed, they also say it is now possible to durably coat a variety of different materials using the same polymeric molecule.

The team accomplished this by developing a molecule with a long backbone from which hydrophilic side chains branch out and impart the non-fouling properties. The polymer also features two types of side chains for covalent bonding to metals — one for binding to silicon and glass, the other for binding to oxides of "transition metals," such as titanium and iron.

The team foresees possible future applications primarily in biomedical diagnostics and medical technology, such as in biosensors, implants and yet-to-be-developed implantable drug-delivery systems. Other applications, they add, could include biofouling prevention in water treatment, ship transport and fishing, as well as in packaging for the food industry.

According to the university, Susos has submitted a patent application for the polymer.

PSN TOP 10 *(as of Mar. 31)*

1. Sherwin-Williams, Valspar Make $11.3B Deal
2. Dow to Settle $1B Class Action Suit
3. Merger to Form Energy Pipeline Giant
4. OSHA: $105K for Fatal Bridge Accident
5. OSHA OKs Long-Delayed Silica Dust Rule
6. Design Changes Eyed in Bypass Collapse
7. Work Site Barges Involved in Fatal Crash
8. Oil Slump Leads to U.S. Steel Layoffs
9. PPG Rebranding Reflects Evolution
10. Tnemec Names Bean President, COO

STUMPER OF THE MONTH

(PSN Daily Quiz, Mar. 14)

True or false: Most sling psychrometers are less accurate than most electronic psychrometers.

True 43% False 57%

Answer: True

About the Answer: So says Alison Kaelin in her February 2016 *JPCL* column, "Office to Field: Lost in Translation—Inspection Equipment."

WHAT'S GOT US TALKING

"Forth Road Bridge Failure Not 'Foreseeable'" *(March 24)*

An inquiry into the December closure of Scotland's Forth Road Bridge determined that the fault could not have been foreseen.

Regis Doucette: "Nonvisible contaminants are not unforeseeable. This looks the same as it did eight years ago when surface chloride ions were determined to be at fault on this same structure. The issues associated with that small pin that seized and prevented proper movement of the structure critical assemblies, to my thinking, seems preventable."

Wayne Senick: "Focusing the right chemistries that have the field-proven properties to free up the pins, stop further corrosion and let them work properly in this case could have saved the taxpayers and the travelling public millions of dollars and a ton of inconvenience, plus reduced the risk of a failure. By drilling down to the issue and approaching the problem as a preservationist, situations like this can be avoided."

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On Sealer Coats for Concrete Surfaces in Marine Environments

IS A SEALER COAT ALWAYS REQUIRED FOR COATING SYSTEMS APPLIED TO CONCRETE SURFACES THAT WILL BE EXPOSED TO MARINE ENVIRONMENTS?

Stephen Bothello
Jotun UAE Ltd.

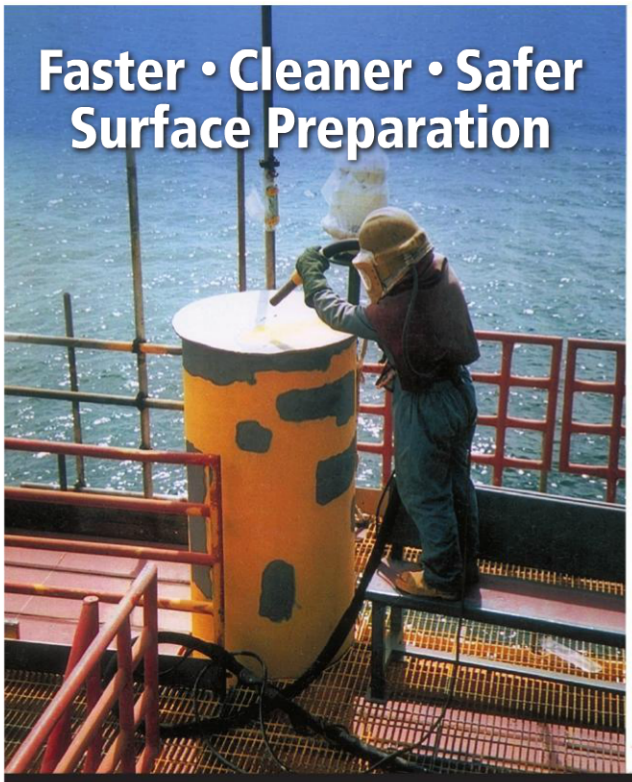
The main factors influencing durability of concrete in a marine environment are 1. porosity, which is an intrinsic property of concrete (interconnected pores); 2. exposure to aggressive elements present in sea water, namely chlorides, sulfides and alkali ions; and 3. exposure to external environmental water (with dissolved salts). If any one of these factors is absent, deterioration of concrete will not occur. Protective coatings applied well over concrete will function to protect the concrete by forming an effective physical barrier to salts, chemicals and external water, thereby enhancing the durability of the structure. However, many coatings due to their design limitations (wetting and penetration properties) will not penetrate into the porosity of concrete and are restricted to physical bonding at the surface level. Also, such coatings applied directly over unsealed concrete will tend to exhibit numerous coating application defects like pinholes and bug-holes that may further lead to premature coating failures like blistering and delamination of the coating, exposing the concrete to the marine environment and finally affecting the durability of the structure.

Om Prakash Jat
Tech International

Concrete is a porous material that readily absorbs liquids. In freeze-thaw climates the expansion of frozen liquids can destroy the surface of unsealed concrete. Oil, salt and other chemicals can discolor and damage unsealed concrete. We must apply a sealer coat to prevent premature failure.

Mario Colica
Colimet S.r.l.

Sealers do not solve the problem. I'd better refer to the decision taken by the New Jersey Department of Transportation in 2001 to use pure zinc metallizing. The metallizing work was performed off-site and no rust problems appeared until now.



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Office to Field: Lost in Translation

SUBCONTRACTING

BY ALISON B. KAELIN, CQA, ABKAELIN, LLC

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The federal government defines a subcontract as "any contract entered into by a subcontractor to furnish supplies or services for performance of a prime contract or a subcontract."

We subcontract two basic things: materials and services.

Material purchases include coatings, abrasives, equipment and laboratory testing and are generally something whose characteristics can be defined, for example, "recyclable abrasive capable of producing a surface profile of 2-to-3 mils," or "meets ASTM D2369." Materials can often be identified by product number. For certification, there may be a reference to meeting ASTM D4537 or SSPC-QP 2.

Subcontracting of services can encompass everything from providing the same services as the prime contractor (such as surface preparation and/or coating application) to any subset of project services, including but not limited to: monitoring, traffic, containment design, containment system erection, movement and removal, or other specialized services such as quality control or quality assurance inspections.

In the case of subcontracting service provision, it is critical that the owner, prime contractor and subcontractor all have a clear understanding of the expectations for execution, oversight, "flow-down" (also referred to as flow-through or pass-through) and ongoing communication with subcontractors throughout the process.

OFFICE

Some owners have prequalification requirements for surface preparation, painting, and hazardous coatings removal work. Prequalification requirements are established by individual states (or other entities) as a minimum attribute of a prime or subcontractor in order to bid work. For example, the U.S. Army Corp of Engineers and other government sectors such as NAVFAC, as well as over half of the U.S. State Departments of Transportation require SSPC-QP 1 and/or -QP 2 certification for contractors performing hazardous coating removal projects. Many of these requirements may also extend to subcontractors. Similarly, various owner contracts and specifications require subcontracting of certified industrial hygienists (CIH), professional engineers (PE), and other services provided to personnel meeting specific qualifications or experience.

In nearly all cases, owners have an expectation whereby obligations and responsibilities owed to the owner by the contractor flow-down to any subcontractor(s). This includes and may state that any expectations, all prequalification requirements and the prime contract documents i.e., drawings, specifications and standards pertaining to the subcontractor's services are

part the subcontract. It may say that this should be the case, but is it happening?

Owners should verify that they are notified of all subcontractors enlisted on a project by the contractor in advance, and in writing. Owners should verify subcontractor qualifications and their abilities to meet both prequalification and project requirements. For example, some large contracts require disclosure of OSHA or other violations and may prohibit bidding by contractors with poor safety records or high EMR rates. The owner should verify that subcontractors are held to these same requirements.

Additionally, subcontractors should be part of the submittal review process prior to the start of the project through the review of qualifications, resumes and programs. However, pre-project submittal review is not enough. Owners should also verify that subcontractors who are providing surface preparation and coating application services or quality, safety or testing services are meeting project requirements in the field initially, and periodically throughout the project.

For example, are the same inspectors performing QA or QC on the project in June as were submitted in March? Are laboratory services being contracted through the accepted laboratory and do the reports contain the information required by the specification? Does the surface preparation and coating application subcontractor have the project specification and is it the current version? Do they have the latest revisions (2015) of critical standards such as SSPC-PA 2, Guide 6 and AB 1?

The following issues should also be verified about subcontractors, based on the complexity of the project.

Management of the Project

How will subcontractor personnel be supervised and who will supervise them? How will quality and safety issues be identified, reported and corrected between the owner, contractor and subcontractor? This is where most misunderstandings arise.

Liability for Delays

Which party will be liable for delays due to staffing issues, equipment problems or bad weather?

Dispute Resolution

Disputes may arise between the owner and the subcontractor or the subcontractor and the prime contractor. How will they be resolved?

Nonconformances and Corrective Actions

Who will initiate, review, disposition and document closure of nonconformances and corrective actions?

Safety

Compliance with OSHA requirements, safety programs, training and monitoring involves anyone in the field or anyone performing production work and can vary greatly. Do third-party inspectors entering hazardous coating removal containments have training, respiratory clearance and blood lead testing? Will the contractor or subcontractor provide the qualified or competent person? When hazards may affect multiple employers such as host, controlling and creating employers, careful planning, communication and hazard evaluations are required.

Documentation

Who is responsible for determining the required, format and content? How frequently does it have to be provided and to whom?

Communication

Which party is responsible for dissemination of requests for information, changes to specifications or requirements, and other project information?

FIELD

Contractors

Contractors working under most quality programs such as ISO, SSPC-QP 3 and nuclear quality programs are generally required to have procedures in place for evaluating their subcontractors, initiating written subcontractor agreements or purchase orders, providing initial and revised documents to subcontractors, and monitoring subcontractor performance on a periodic basis.

Subcontract agreements should identify the scope of services, flow-down of owner requirements, specifications, standards and contract documents. They should also identify requirements for the following if specified or applicable.

- Project submittals
- Schedules and coordination
- Qualifications of staff
- Inspection equipment and calibration
- Materials and material testing or certification
- Project documentation

In addition, consider how safety issues will be coordinated and whether or not safety plans or training are required. Unique owner or project requirements should also be identified and clarified.

Many projects require data or record entry onto clouds or other document-management systems. Large projects may require daily quality assurance entries, management of all submittals and reviews, non-conformances, and all project communication through a singular system. It is important to clarify how subcontractors will access or use any relevant systems.

The contractor must flow-down all owner and contract documents, including revisions, to any affected subcontractors performing services.

Subcontractors

Put simply, know what you are committing to do and that you have the ability to do it. You should review the prime agreement between the owner and the contractor, as well as the specifications, to verify that you can provide the agreed-upon services. Don't just look at the coating application specification; obtain and review all other referenced sections. For example, Unified Facilities Guide Specifications (UFGS) 09 97 13.27,

"Exterior Coating of Steel Structures" may incorporate Section 02 83 13.00 20, "Lead in Construction." Check both the DOT standard specification and any project-specific special provisions.

As an example, the standard specification may require surface preparation to SSPC-SP 10 and application of a primer at 3-to-5 mils dry-film thickness. However, the special provisions require application of an additional 3-to-5 mils of primer on the outside fascia.

If standards and testing are referenced, make sure you understand what they entail. The above example requires an SSPC-SP 10, "Near-White Blast Cleaning." Do you understand that SSPC-SP 10 incorporates by reference the latest revision of SSPC-SP 1, "Solvent Cleaning" and SSPC-AB 1, 2 or 3 (as applicable)? SSPC-AB 1, "Mineral and Slag Abrasives" requires testing of the abrasive prior to use and periodically for conductivity to ASTM D4940, "Standard Test Method for Conductimetric Analysis of Water Soluble Ionic Contamination of Blast Cleaning Abrasives" and ASTM D7393 "Standard Practice for Indicating Oil in Abrasives." As a subcontractor, you would be expected to perform and document the required testing as defined in the applicable standard(s).

Subcontracting can be a very effective arrangement but all members of the process from the owner, to the contractor and the subcontractors, must establish clear expectations and communication throughout the process to avoid conflict and meet the contract requirements.

ABOUT THE AUTHOR

Alison B. Kaelin, CQA, has more than 30 years of public health, environmental, transportation and construction management experience in the coatings industry. She is the owner of ABKaelin, LLC, a provider of training, quality assurance, auditing, consulting, and related services to the protective coatings, construction, fabrication and nuclear industries. Kaelin is a certified quality auditor and a NACE-certified Coating Inspector. She was a co-recipient of the 2016 SSPC Coatings Education Award and the inaugural SSPC 2014 Women in Coatings Award, a 2012 *JPCL* Top Thinker, a 2012 *JPCL* Editor's Award Winner and an SSPC Technical Achievement Award winner in 2005. Kaelin is also a *JPCL* contributing editor.

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Fig. 1: This photo demonstrates that not only are there cracks and delamination present, but it provides clear evidence of water intrusion. The visible rust stains are only found when ferrous metals begin to corrode. All figures courtesy of the author.

Failure at the Jail

By Richard A. Burgess, PCS, Series Editor, KTA-Tator, Inc.

This article describes the failure of coatings applied to the concrete floors of a county detention facility. The structure is a multistory building with modular cells and concrete floors on each inmate level. The main structural floors were constructed of lightweight aggregate and the mezzanine floors constructed of a normal-weight aggregate. The facility was relatively new, becoming operational in September of 2015. The building could accommodate 608 inmates with the ability to add additional space as the population demanded.

The coating failure, which revealed itself via delamination, cracking, heaving and crumbling, began within a few months of opening, and presented in both isolated and progressive failures on multiple levels but appeared to be predominantly present on the mezzanine floors. Repairs were originally performed using coating materials supplied by the original product supplier. Later, epoxy topping repairs employed materials from a different supplier for test applications. The failures continued, prompting this investigation.

The engineering firm elected to conduct the field investigation themselves

and the investigator was provided with background and technical information, photographs and intelligence from the observations of others. Core samples representative of failing and non-failing floor surfaces were requested by the investigator. The core samples were removed from four locations selected by the client and included two failing and two non-failing samples. Since the samples were selected based on whether the coating was failing or non-failing, the failing samples were both removed from the same floor type (mezzanine) and the non-failing samples from the main structural floor. The possible influence of floor type was not considered.

The Investigation

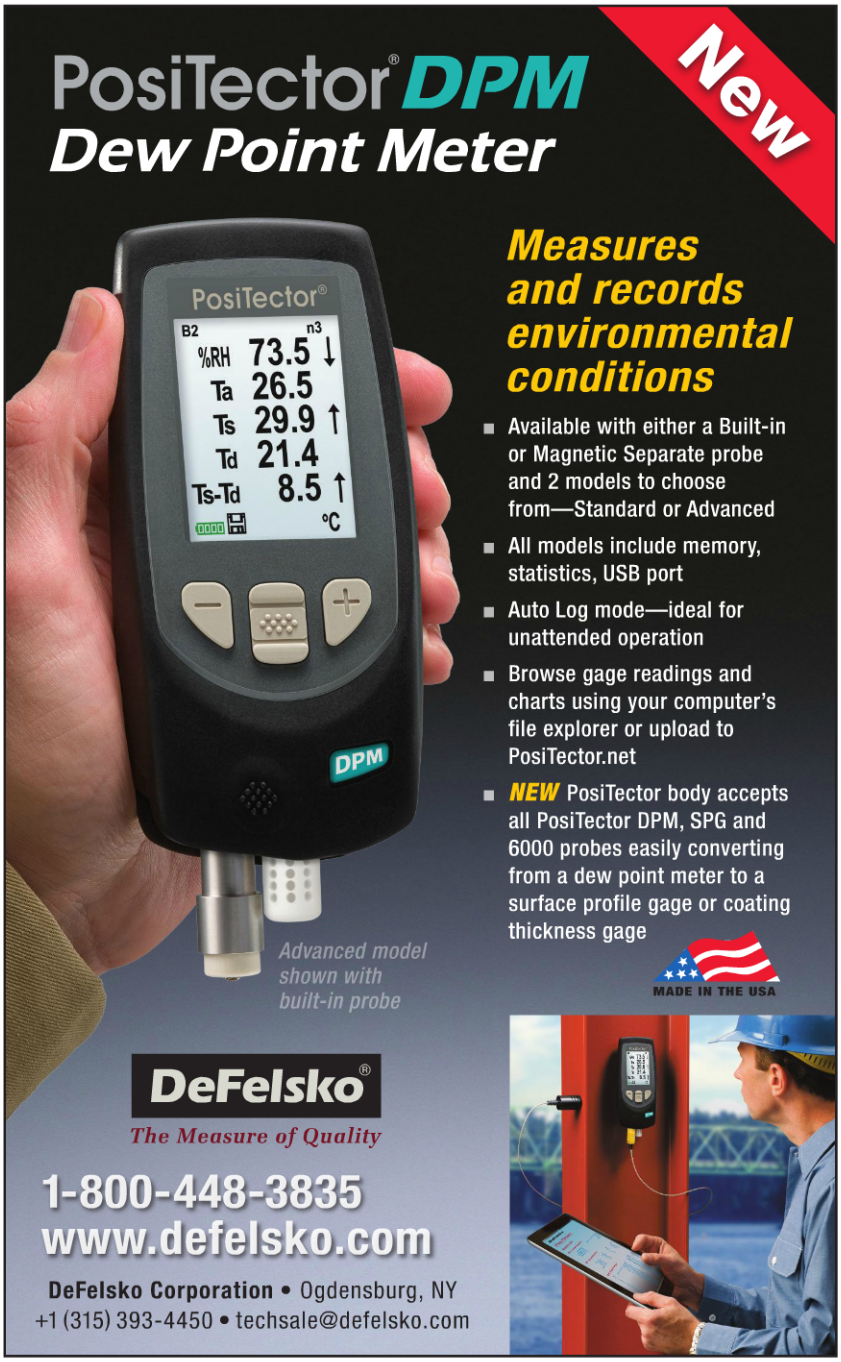
Not having the opportunity to conduct a site investigation can be a handicap as a great deal of information can be determined through that process. It is possible that some details of the surface preparation and coating application methods could've been discerned leading to reasonable hypotheses as to possible causes of the failure.

For example, testing for the presence of water beneath delaminated coating could've been conducted, as well as judging the difficulty of removing the coating from the substrate or separating coating layers in the field. Such information will generally provide more meaningful information than that evaluated in the laboratory. In fact, field findings are commonly the basis used for determining which initial laboratory examinations should be undertaken so that hypotheses as to the failure from the field investigation can then be tested through sample analysis.

Reviewing the Available Information

Supplied background technical information coupled with site photographs assisted in the determination of a specific path to follow. An epoxy flooring system was originally specified consisting of four distinct coating layers: a primer at 6-to-8 mils, an aggregate-filled base coat to a nominal 250 mils (1/4 inch) a grout coat at 8-to-32 mils (to seal the base coat) and a topcoat at 6-to-8 mils. The intent of the work, per the project specification was "… to provide a seamless epoxy floor and base." The system design also included:

- a waterproofing membrane (to be furnished by the manufacturer of the epoxy) that was a trowel-applied, reinforced latex composition strengthened with glass mat or plastic fabric (or equal);
- a specified aggregate; well mixed throughout the thickness of the flooring (not sprinkled or broadcast on top); and



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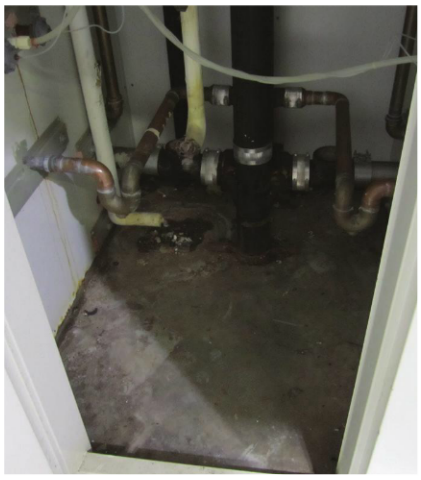

Fig. 2: It is imperative to locate the source of even small leaks as they can often lead to significant moisture issues within a coating system.

• a transition coat of homogeneous methyl methacrylate (acrylic).

Surface preparation specs dictated preparing concrete surfaces by removal of laitance and other interference materials. Installation requirements consisted of:

• coating the floors and wall bases with troweled adhesion waterproofing;
• embedding the reinforcing fabric; trowel- or brush-applying additional waterproofing coating to seal voids;
• applying a thin coat of the manufacturer's standard bond coat; and
• trowel-applying the resin and aggregate mixture at a thickness of 1/4-inch.

Photographic Evidence

Numerous photographs were provided showing the nature and distribution of the detention facility floor coatings. Because moisture is a common problem with concrete surfaces the photographs were reviewed for evidence of this possible cause (Fig. 1, p. 16).

When free water is contributing to a coating failure it is important not only to demonstrate this through the evidence but also to seek out the source. The engineering firm was encouraged

to look for likely sources of free water and one source not to be ignored is floor cleaning. Since cracks were present it is possible that cleaning water seeped through cracks to the substrate. While this theory could not be ruled out, Figure 2 shows that even small leaks can be significant sources of water.

In addition to the evidence confirming that water was contributing to the coating failure, another likely contributing factor was observed in the photographs — the crumbling of a thick monolithic cementitious layer, shown in Figure 3 (p. 20). The integrity of a concrete layer and the aggregate-filled epoxy coating layer is clearly compromised. This issue could be related to the concrete itself being a non-durable substrate, or the result of a coating issue related to the aggregate-filled coating material. Cracks in the coating can result from moisture vapor pressure contributing to coating delamination. Rigid aggregate-filled films do not recover from deformation. They are subject to flexure when loose which induces cracking. A site visit could have served to clarify the initiating factor.

Core Samples

The four concrete core samples requested were provided to the investigator. The two from failing areas and two from non-failing areas (used as control samples) are described in Table 1 (p. 18).

Visual and Microscopic Examination

In addition to visual inspection, microscopic examination of the samples was conducted using a digital microscope with a magnification to 200X. Findings from the handling and examination of the cores follow.

Sample 1: Non-Failing Core Sample Findings

- The coating could be pried from the edges of the core and delamination could be forced with gentle pressure.
- The plane of failure appeared to exist between the layer with the colored aggregate and another layer with a translucent coating and clear aggregate that remained tightly adhered to the core sample.
- The colored aggregate layer showed encapsulation by resinous material when the cross-section was viewed.

Sample 2: Failing Core Sample Findings

- The sample arrived with coating material completely delaminated from the concrete substrate.
- The plane of separation (failure) was between the colored aggregate layer and a translucent layer that was present on the concrete.

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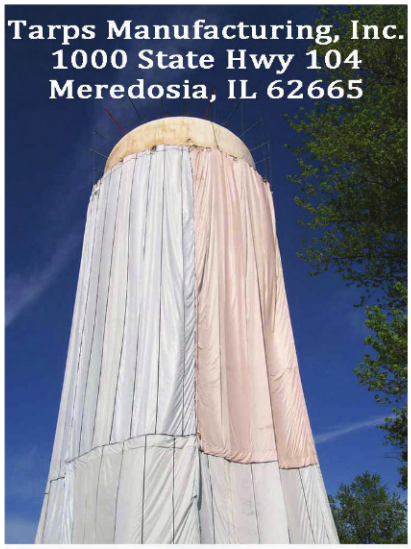

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Fig. 3: The failure mechanism also suggests areas without a durable substrate.

Fig. 4: This photo shows evidence of standing water which has stained the cell floor. Cracks in the coping around the base of the walls allow the water to enter.

- In one small area the translucent layer was noted as present on the back of the delaminated chip.
- The clear aggregate associated with the translucent layer was significantly less dense on this sample as compared to Sample 1.
- The colored aggregate appeared more densely packed in this sample as opposed to Sample 1 with less encapsulation by the resinous material.
- The surface of the substrate was viewed and revealed the translucent material as well as a white powdery material that was likely from the resin of the colored aggregate layer.

Sample 3: Failing Core Sample Findings

- The coating material was delaminated from the top surface upon receipt by the laboratory.
- The plane of failure was between the colored aggregate and the translucent and fibrous material that appeared to be a surface layer over the concrete.

TABLE 1: CONCRETE CORE FLOOR AND COATING SAMPLES

Non-Failing Samples			Failing Samples		
Sample	Location	Floor Type	Sample	Location	Floor Type
Core Specimen #1	Level 10 South Cell 10B05	Main Structural Floor Light Weight Aggregate	Core Specimen #2	Level 10 South Cell 10A44	Mezzanine Normal Weight Aggregate
Core Specimen #4	Level 6 North Cell 6D01	Main Structural Floor Light Weight Aggregate	Core Specimen #3	Level 8 North Shower	Mezzanine Normal Weight Aggregate

• The colorfed aggregate in this sample could not be loosened as easily as with previous samples. In one area the colored aggregate layer left a spot with aggregate and a powdery residue on the concrete core when removed.

• The top surface of the coating material was viewed and a few cracks were noted through the topcoat. The aggregate

• The top surface of the delaminating coating chip had many cracks in the top surface of the clear coat.

• In some areas the colored aggregate appeared to be protruding through the clear coat.

• The bottom surface consisted primarily of the colored aggregate but also showed a white, powdery residue present between the spaces and along the aggregate, but did not encapsulate the aggregate.

• The colored aggregate could be loosened by gentle scraping.

• The colored aggregate was observed in the clear topcoat, suggesting that it was loose at the time of the clear coat application.

• There was minimal resinous material present in the layer filled with colored aggregate.

• The area under the delaminated coating showed fibers, a translucent material, and a material that was cementitious. Voids were noted through this cross-section.

Sample 4: Non-Failing Core Sample Findings

• The coating material could be lifted and delaminated from the edges of the core using moderate pressure from the fingertips.

• The plane of failure appeared to be between the colored aggregate layer and a clear aggregate with a possible translucent (primer) resin binding it to the concrete surface.

• When probed with a knife, the clear aggregate could not be easily removed, but the colored aggregate could be loosened.

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was completely covered by the topcoat and was not protruding through the top surface.

• The bottom surface of the delaminating coating material consisted of a colored aggregate material and appeared to have some resinous material present. This sample was significantly less powdery than previously examined samples where the aggregate was more loosely adhered.

• The area under the delaminated coating material left behind from the filled coating layer was some colored aggregate and some powdery residue.

• The clear aggregate did not appear to be encapsulated by the translucent material as seen in Sample 1. The colored-aggregate layer showed an increased presence of binder material around the colored aggregate.

In addition, a waterproofing membrane system (with imbedded fiber) was called for in the specification but only found in Sample 3. There is no indication as to why the waterproofing membrane is absent at the other core locations.

Infrared Spectroscopy

Infrared spectroscopic analysis was performed in the lab. This technique involved combining sample scrapings with potassium bromide powder and forming pellets under high pressure. The pellets were then placed in the optical path of the spectrometer and spectra were obtained over the range of 4,000-to-400 cm^{-1}. The clear topcoats from Samples 1 through 4 were consistent with epoxy resin.

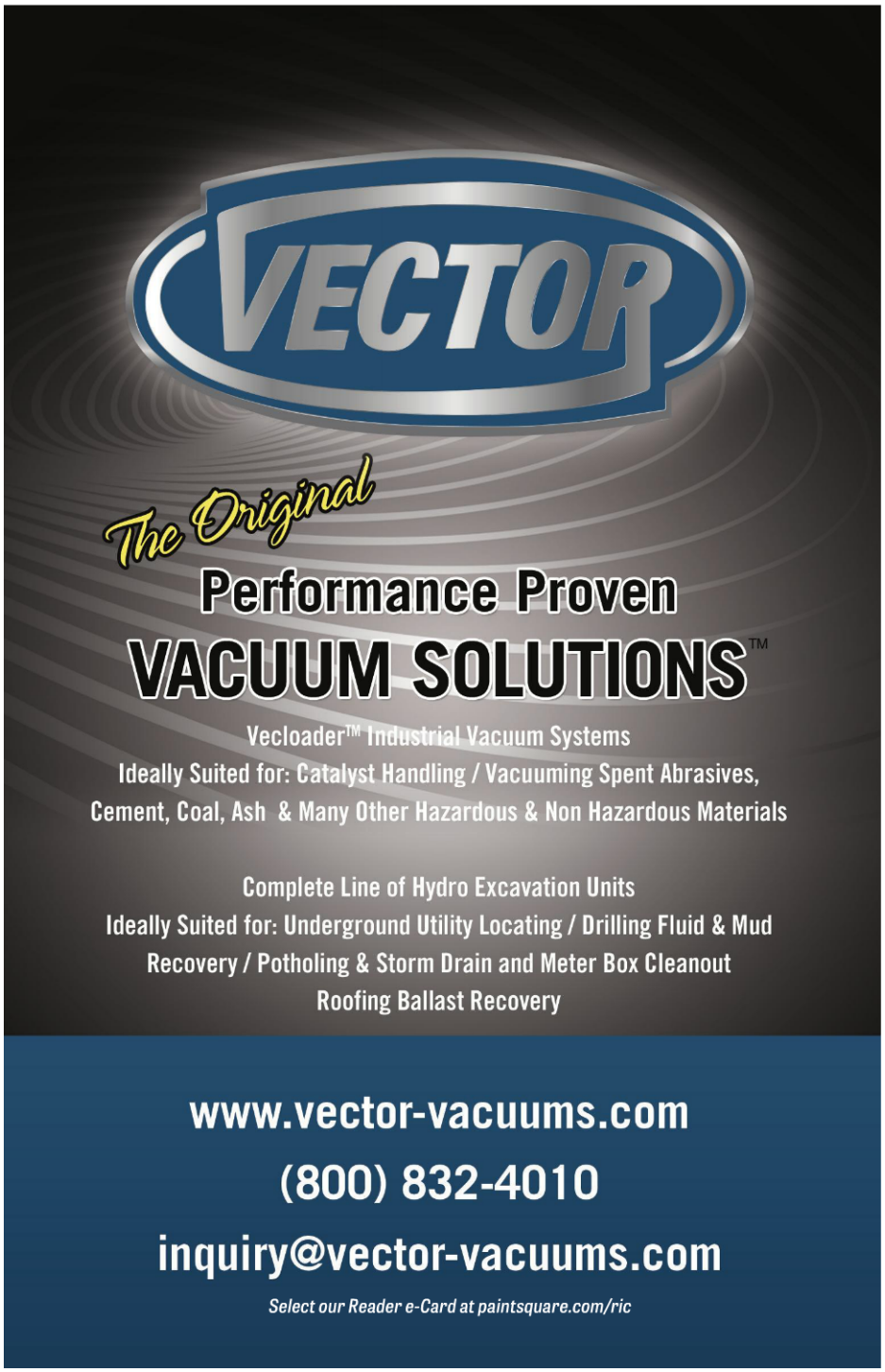
The white powdery residue attributed to the layer filled with colored aggregate was consistent with crystalline silica. No evidence of resinous material was indicated by the spectrum produced by the powdery residue, which was present at the failure interface.

The translucent material on the bottom surface of Sample 2 was consistent with an epoxy resin. The presence of calcium carbonate was also indicated.

No methyl methacrylate (acrylic) coating layer was found.

Summary

The coating system applied to the concrete of this facility was not the one specified. A four-coat system was specified and only two distinct coating layers were present in the core samples. The waterproofing membrane specified was only found to be present in one of the four core samples and although only one aggregate was specified, two (colored and clear) were found to be present. A



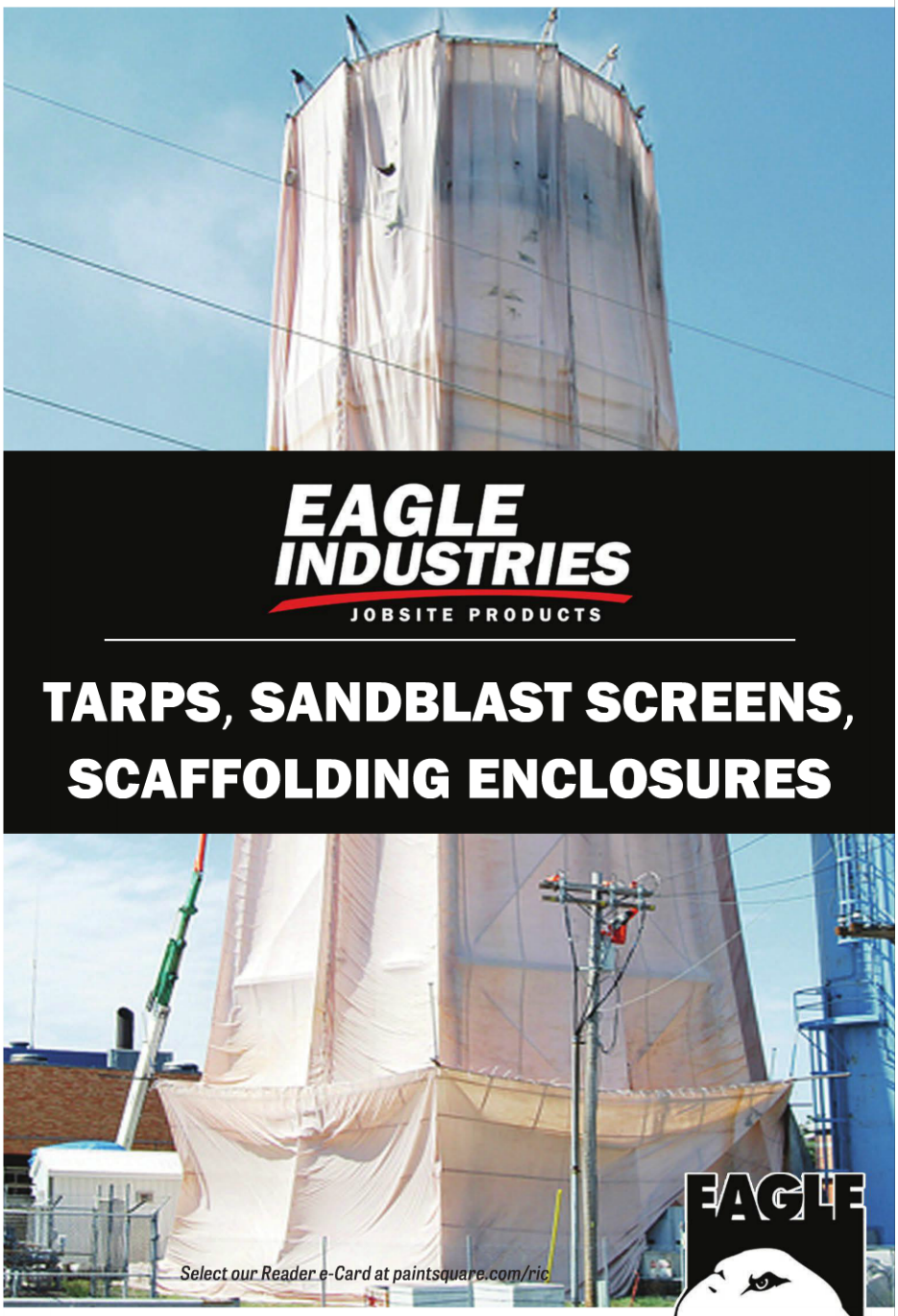
plane of weak intercoat adhesion was noted in the field as delamination was found between a coating layer filled with colored aggregate and the subsequent layer on each of the samples submitted. Two concrete core samples from non-failing locations arrived with the coating system intact, but it easily separated with mild pressure. The two concrete core samples from failing locations arrived with the coating system separated.

Some of the coating failures were associated with moisture intrusion as evidenced by delamination with rust staining and efflorescence in coating cracks visible in the photographs provided (Fig. 3). It was reported that common plumbing chases between adjacent cells include water and waste connections. Leaks in these locations provide a path for water to travel along the concrete to the underside of the floor coating system. Once the moisture entered the concrete layer it migrated over a large area before showing evidence of its presence.

There was also evidence that underlying layers, cementitious and aggregate-filled, had crumbled along delaminated edges. This was a progressive deterioration most likely brought about by loading on a non-adhered edge when there was flexure of the delaminated layer. There were insufficient layers and the layers present were not at the appropriate thickness or consistency of binder-to-aggregate ratio, possibly due to mixing differences from location to location. This is supported by the failure of the aggregate to fully integrate into the binder.

Conclusion

This case from the F-Files was presented to address the clues uncovered through examination of contract documents, information from on-site personnel, photographs depicting the condition of the coating and evidence of failure, and examination of coating samples, as there was no opportunity to visit the project site to conduct a hands-on examination. However, it was still possible to develop an understanding of what had occurred in the field and verify those theories through laboratory testing determining that the initial coating application, binder and aggregate mixing were flawed, the required film thickness was not achieved and the specified coating system had not been installed. It is not clear whether or not a substitution of materials was permitted. It is clear, however, that the system as installed was faulty. Add moisture to the equation and this flooring system was doomed to failure.



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Coating Industry's Best Honored at SSPC 2016

By Amy Woodall, PaintSquare News

On Jan. 18, SSPC kicked off SSPC 2016 in San Antonio with the annual Awards Luncheon, celebrating the best painting projects and papers completed in the previous year, as well as honoring those outstanding individuals whose careers reflect decades of service to the industry.

SSPC STRUCTURE AWARDS

Complex industrial coatings projects displaying outstanding aesthetic merit and craftsmanship were among those recognized with SSPC Structure Awards. These awards are given to recognize the work of teams of contractors, designers, end users and coating manufacturers for excellence on particular coatings projects.

ERIC S. KLINE AWARD

The inaugural Eric S. Kline Award was created this year to recognize outstanding achievement in industrial coatings work performed in a fixed shop facility. The project can involve repair work or new construction.

Fore River Bridge

Structure Owner: Massachusetts Department of Transportation (MassDOT)

Contractor/Applicator: Fletch's Sandblasting & Painting Inc. (SSPC-QP 1-, -QP 2- and -QP 3-certified)

Coating Material Supplier: Carboline Company

Painting the replacement lift span of the Fore River Bridge between Quincy and Weymouth, Mass., was an important project for Fletch's Sandblasting & Painting, as it was the first project of this size and the first project with MassDOT for the company. Earlier MassDOT projects were limited to primer only and smaller components; Fletch's was the only company willing to take on the task of applying a three-coat system and paint the inside of the box members, a very tight space.

The lift span of the bridge contained about 500 tons of box members. There were just over 120 box members in total, of various sizes and lengths. The company knew attention to detail would be critical, and so the crew stripe-coated all the bolts, welds and inner diaphragms before each coat, even though it was not required in the specification.

The team made every attempt to go the extra mile because of how much it had riding on this project — it was about more than doing a job to be paid for. The company sought to make a statement that it was not only willing to do the work specified, but also what it took to ensure the best results.

The project began in March 2014 and was completed in September 2014. As of press time, the lift span is constructed and will stay in Quincy until the towers on either side of the river are complete with the approaches. At that time the lift span will be floated downstream and locked in.

(Above left, bottom left): Steel box members of the Fore River Bridge were shop-coated, assembled, and delivered to the site for installation. Photos courtesy of Fletch's Sandblasting & Painting Inc.

(Above right, L-R): Curtis Fletcher, production manager, and Sean Fletcher, business development manager, Fletch's Sandblasting & Painting Inc.; Deborah Simmons, senior market manager, Carboline Company; and Michael Hewins, coatings engineer, MassDOT. All award ceremony photos courtesy of SSPC.

WILLIAM JOHNSON AWARD

The William Johnson Award recognizes outstanding achievement in aesthetic merit in industrial or commercial coatings work. Criteria include color, gloss, texture and how the coating complements the environment while enhancing the structure. The award is named for the late consultant with KTA-Tator Inc., whose work in coatings formulation, failure analysis and surface preparation was instrumental in advancing the industry.

Imperial Sugar Elevated Water Tank

Structure Owner: The Johnson Development Company

Contractor/Applicator: Blastco/TF Warren Group (SSPC-QP 1-, -QP 2- and -QP 3- certified)

Coating Material Supplier: Tnemec Company, Inc. (The Barry Group)

(Top): The 200-foot-tall Imperial Sugar water tank received a new coating system and upgrades to withstand strong Gulf Coast winds. Photo courtesy of Dunham Engineering.

(Bottom, L-R): Stephanie Elko, environmental and corrosion engineer, Dunham Engineering, Inc.; Jay Soper, president, Blastco Inc.; Paul Higgins, The Barry Group; Jerry Ulke, development manager, The Johnson Development Corp.; Travis Tatum, president and CEO, and Shawn McIlvain, supervisory inspector, Dunham Engineering, Inc.

This 200-foot-tall steel water tower at the Imperial Sugar Refinery has dominated the skyline in Sugar Land, Texas, for more than 75 years. Holding 100,000 gallons, the riveted steel water tank provided potable water to the physical plant and sugar mill until the mill closed in 2003.

The years of idle service and lack of maintenance were not kind to the steel structure — more than 75 percent of the surface area was severely corroded, leaving it vulnerable to the hurricane-force winds routinely experienced in the Gulf Coast area.

This project involved replacing wind rods and extensive welding work on the tank balcony and roof connections. Because the tank was removed from service and water would no longer be stored in the tank, sand was added to the lower portion of the tank to serve as ballast and help anchor the structure to the ground.

All of the structure's protective coatings were replaced with new fluoropolymer coating systems, which provide corrosion protection and restored this landmark of the Sugar Land skyline. The exterior finish coat was selected to provide an antique finish and preserve its historic appearance.

GEORGE CAMPBELL AWARD

The George Campbell Award honors a difficult or complex industrial or commercial coatings project. Challenges may include extreme environmental conditions, time constraints, limited access or high traffic, complex structural components, or coordination with multiple trades or subcontractors. The award is named for the late founder of Campbell Painting Company in New York.

Lay Dam

Structure Owner: Alabama Power Company
Contractor/Applicator: Vulcan Painters, Inc. (SSPC-QP 1-, -QP 2- and -QP 3-certified)
Coating Material Suppliers: International Paint/AkzoNobel, The Sherwin-Williams Company and PPG Protective and Marine Coatings

This makeover and update of a 100-plus-year-old multi-level hydroelectric plant on the Coosa River in Alabama included coatings work inside and out. Intended to bring the plant up to an "extraordinary level" of safety, it also sought to add corrosion protection and visual appeal. To look its best for the centennial celebration and to improve its efficiency, the plant needed an overhaul, including an upgrade of two of its turbines, a new roof on the powerhouse and new paint throughout. Work included grinding to achieve an ICRI CSP-3 profile; patching and coating plant floors; and applying non-skid coatings to the shop floor. Additional work focused on coating interior and exterior cranes, walls, stairways, machinery and railings. Scaffolding was built to reach painted surfaces, and the exterior crane and other areas were de-leaded as necessary (using SSPC-QP 2 protocol). Painters used a man lift to wash the cornice of the powerhouse; the brick façade; and the powerhouse's hollow metal doors and frames. Footbridges on the property were rigged and pressure washed.

(Top): The century-old Lay Dam power plant went through an extensive rehabilitation, including application of a new interior floor coating system. Photos courtesy of Vulcan Painters, Inc.

(Bottom, L-R): Doni Riddle, VP global accounts, The Sherwin-Williams Company; Al Kaminsky, zone manager, PPG Protective and Marine Coatings; Roger Yeargan, hydro manager, Alabama Power; Steve Feldman, director of sales, U.S. and Canada, PPG Protective and Marine Coatings; and Doug PiGue, assistant project manager, The Vulcan Group.

The crew striped the helicopter pad with reflective paint, striped ramps at the bottom of the dam and painted poles around the entrance. They also pressure-washed curbs and applied striping and stenciling. At Vulcan Painters' suggestion, railings inside were painted safety yellow. Fire extinguisher mounting areas were painted safety red for quick identification.

The project was started July 7, 2014, and completed Oct. 16, 2014.

New San Francisco-Oakland Bay Bridge

Structure Owner: California Department of Transportation (CalTrans)
Contractor/Applicator: Certified Coatings Company (SSPC-QP 1- and -QP 2-certified)
Coating Material Suppliers: International Paint/AkzoNobel, Carboline Company, Rust-Oleum Corporation

(Top): Cable painting on the new San Francisco-Oakland Bay Bridge presented a number of access challenges for the contractor. Photos courtesy of David Brockman, Certified Coatings Company.

(Bottom, L-R): Kenneth Matula, industrial sales rep, Kelly-Moore Paint Co., Inc.; International Paint representative; Peter Fisher, manager of technical services, Rust-Oleum Company; Deborah Simmons, senior market manager, Carboline Company; David Brockman, president, Certified Coatings Company; Lucky Vervilles, former VP, coatings division, and Chris Meintanis, vice president, Atlantic Plant Services.

Field painting of the self-anchored suspension (SAS) portion of the new San Francisco-Oakland Bay Bridge, which was done over four-and-a-half years, posed a number of obstacles to crews on the job.

The most challenging came from painting the bridge's cable system. At 350 feet long, the system was installed at a steep angle that crosses above the traffic lanes. Cable painting began in late fall of 2012 with a target completion date of May 2013 to allow the bridge to open by Labor Day 2013. The schedule had to be maintained through winter weather when there was no practical way to contain the cables to control atmospheric conditions. Also, the default method of suspended access, which would roll on the suspenders with a trolley system, would have likely caused significant damage to freshly applied coatings. To avoid damage and accelerate the schedule, an automated cleaning and spraying system was developed. The system also reduced some of the safety hazards.

One of the final challenges was abrasive blasting the 280,000 square feet of exposed steel deck surface on the top of orthogonal box girders and applying a coat of zinc-rich primer prior to paving operations for the road deck. This was completed within three weeks.

The painting effort required coordination with all crews. Success depended on communication and flexibility to shift resources and focus in order to maintain forward momentum despite the challenges. The project began July 10, 2010, and was completed on Dec. 1, 2014.

E. CRONE KNOY AWARD

Named for the late founder and president of Tank Industry Consultants, the E. Crone Knoy Award acknowledges coatings work that demonstrates innovation, durability or utility. It recognizes outstanding achievement that may include excellence in craftsmanship, execution of work, or the use of state-of-the-art techniques and products to creatively solve a problem or provide long-term service.

Facilities Upgrade and Refurbishment Southwest Soldado Oil Fields

Structure Owner: Petrotrin LLC
Contractor/Applicator: Theo Richards General Contractors Ltd.
Coating Material Supplier: Carboline Company

When the Southwest Soldado Oil Fields, located about seven miles off the Venezuelan coast, required a facilities upgrade and refurbishment, the scope of work called for major structural and pipeline repairs. Additionally, crews needed to recoat all structural members of the various oil and gas well structures with a three-coat system.

With structures sitting in at least 80 feet of water, work was carried out from a workboat with about 60-feet-by-40-feet of deck space. The project began on June 8, 2013, and wrapped up July 14, 2014.

(Top): A three-coat system was applied to structures in the Southwest Soldado Oil Fields seven miles off the Venezuelan coast. Photos courtesy of Theo Richards General Contractors Ltd.

(Bottom, L-R): Theophilus Richards, managing director, Theo Richards General Contractors Ltd; and Deborah Simmons, senior market manager, Carboline Company.

CHARLES G. MUNGER AWARD

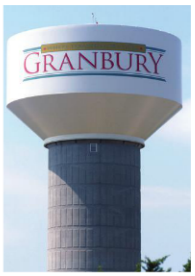
This award honors an outstanding industrial or commercial coatings project that demonstrates the longevity of the original coat-ing. The structure may have had spot repairs or overcoating with the original coating still intact.

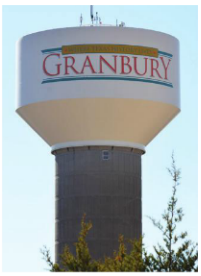
Granbury Composite Elevated Tank

Structure Owner: City of Granbury, Texas
Contractor/Applicator: TASCO Painting, and Landmark Structures (SSPC-QP I-certified)
Coating Material Supplier: Tnemec Company, Inc.

In 2000, a new 500,000-gallon water tank needed a coating system that would last as long as possible. For this project, the tank would be painted bright white, with a painted logo and slogan for a Texas town steeped in tradition.

This upcoming spring will mark 15 years since the water tank in Granbury, Texas, was originally coated with a coating system that included a fluoropolymer finish. The coating of the tank started in late 2000 and was completed in April 2001. Since then, the color and gloss levels on the tank have barely changed at all.





(Top): The Granbury water tank's coating appears virtually the same today (right) as it did when it was originally coated in 2001 (left). Photos courtesy of Tnemec Company, Inc.

(Bottom, L-R): Jeremy Day, coating project manager, Landmark Structures; and Mike Shields, Tnemec independent representative, The Barry Group, LLC.

The 15 years that this coating has remained aesthetically pleasing is almost twice as long as an acrylic polyurethane finish coat, which was the standard topcoat before the fluoropolymer became available. Due to results like this, fluoropolymer has become a standard finish for water tanks and specialty architectural applications around the world in recent years.

This tank's logo includes the town's motto, "Where Texas History Lives." Now, after 15 years without a recoat, this is also where coatings history lives.

MILITARY COATINGS PROJECT AWARD OF EXCELLENCE

The Military Coatings Project Award of Excellence recognizes exceptional coatings work performed on U.S. military ships, structures or facilities.

Sea-Based X-Band Radar

Structure Owner: Military Sealift Command
Contractor/Applicator: Pacific Shipyards International
Coating Material Supplier: PPG Protective & Marine Coatings, Honolulu Store 9434

(Top): Components of the SBX-1, a floating, self-propelled mobile radar station for the U.S. military, were cleaned and recoated. Photo courtesy of Nicholas Olvey, SPCC & Consulting.

(Bottom, L-R): Steve Feldman, national sales manager, and Tom Morrissey, government and marine sales, PPG Protective and Marine Coatings; Nicholas Olvey, SPCC & Consulting; Gene Fukushima, VP business development, Pacific Shipyards International; and Brian Steuerwald, mechanical engineer, Military Sealift Command.

The Sea-Based X-Band Radar (SBX-1) — a floating, self-propelled, mobile active electronically scanned array early-warning radar station — was designed to operate in high winds and heavy seas managed by the Military Sealift Command.

Stationed out of Joint Base Pearl Harbor Hickam, Ford Island, on the opposite side of the USS Missouri Memorial, this project included the abrasive blasting and repainting of the four sea chests, chain lockers, anchor chains and the oily water tank internals.

The SBX-1 has been recoated without complete removal of the coating system using ultra-high-pressure waterjetting. The main vertical columns (port and starboard) were coated in May 2013, and exhibit excellent corrosion protection to date. In 2015 the mid columns and vertical diagonal bracing were coated using similar surface preparation and application techniques and standards.

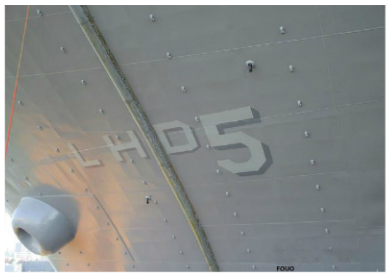
USS Bataan (LHD-5)

Structure Owner: U.S. Navy
Contractor/Applicator: General Dynamics – NASSCO-Norfolk
Coating Material Supplier: PPG Protective & Marine Coatings

When performing the exterior preservation of the *USS Bataan*, including the removal of the existing coating and paint application onto 415,000 square feet, crews faced challenging winter weather in the Norfolk area. Several snowstorms and abnormally cold, below-freezing temperatures impeded work, especially affecting the contractor's ability to perform steel shot and garnet blasting and apply coatings.

Other challenges included integration with the other trades, contractors and the BAE shipyard. A substantial amount of structural repairs and upgrades were also required, and often this other work took precedence over the pblasting and painting activities. The team continuously had to negotiate structural hot work with coatings cold work, which would result in the suspension of preservation activities. The preservation group had to work an accelerated schedule while maintaining quality and safety standards.

Additionally, more than 36 tanks with approximately 600,000 square feet of surface area and the ship's welldeck were preserved concurrently with



(Top): Fresh coatings were applied to 415,000 square feet of surface area on the U.S. Navy's USS Bataan. *Photo courtesy of BAE Systems Shipyard, Norfolk, Va.*

(Bottom, L-R): Jeffrey Hall, PPG Protective and Marine Coatings; Jon Russell, Coatings Deputy Program manager; and Phil Avery, coatings quality assurance manager, General Dynamics-NAASCO-Norfolk; and Tom Morrissey, government and marine sales, PPG Protective and Marine Coatings.

the underwater hull and freeboard. On top of mitigating contamination issues with blasting and painting the hull while blasting and painting the tanks, each of the 36 tanks required 78 temporary access openings (TAOs) be cut into the hull to allow ventilation and egress for tank coating. After completion of tank preservation, these TAOs were welded back into place and then blasted and painted as a new portion of the underwater hull zone.

The project began on Jan. 26, 2015, and was completed Oct. 13, 2016.

SSPC INDIVIDUAL AWARDS

SSPC also honored a variety of coating industry leaders at the awards luncheon, as well as outstanding SSPC chapters and college scholarship recipients.

The **HONORARY LIFE MEMBER** is a title bestowed upon an individual by the Board of Governors for extraordinary long-term activity on behalf of SSPC. Nominees for this award are individuals who have spent their careers

dedicated to the coatings industry and have spent considerable time and effort advancing the causes of SSPC. To achieve this recognition, nominees must receive approval from two-thirds of the SSPC Board. The 2015 Honorary Life Member is **Alfred Beitelman**, retired from the U.S. Army Corps of Engineers Construction Engineering Research Laboratory.

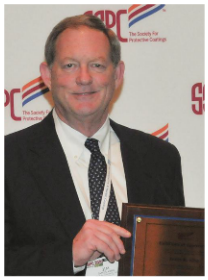
Named for SSPC's executive director from 1957 to 1984, the **JOHN D. KEANE AWARD OF MERIT** acknowledges outstanding leadership and significant contribution to the development of the protective coatings industry and to SSPC. This year's recipient is **Stephen Cogswell**, BAE Systems Southeast Shipyards.

The **WOMEN IN COATINGS IMPACT AWARD**, SSPC's newest individual award, was established at SSPC 2014 to recognize women in the coatings industry whose contributions

have created a positive impact on the culture of the industry. This year's winners are **Lydia Frenzel**, Advisory Council and **Gail Warner**, retired from Huntington Ingalls – Newport News Shipbuilding.

The **COATINGS EDUCATION AWARD** is given for significant development and dissemination of educational material and technical information relating to protective coatings and their application. This year's recipients are **Alison Kaelin**, ABKaelin LLC and **Terry Gabbert**, LEIDOS.

The **TECHNICAL ACHIEVEMENT AWARD** recognizes outstanding service, leadership and contribution to the SSPC technical committees and other special technical functions. This year's award was presented to **Kristin Leonard**, Bechtel Corp.



Whenever SSPC's leadership changes, the Society presents a **PAST PRESIDENT AWARD** to honor past presidents for their years of service and leadership provided to the Society. SSPC recognized its current immediate past president, **Jim King** of John B. Conomos Inc.

When awarding the annual **OUTSTANDING NORTH AMERICAN AND INTERNATIONAL CHAPTERS**, SSPC considers all of the chapter activities and the overall operation of the chapter. The **Gulf Coast Chapter** and the **Midwest Chapter** received the Outstanding North American Chapter Awards. This year's Outstanding International Chapter Award was given to the **Saudi Arabia Chapter**.

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MOISTURE TESTING IN BELOW-GRADE CONCRETE WASTEWATER STRUCTURES

BY R. A. NIXON, CORROSION PROBE, INC./ CORROSION TESTING LABORATORIES, INC.

When establishing the minimum conditions suitable for coating concrete, many coating manufacturers, and hence specifying engineers and contractors, rely on unrealistic moisture content and moisture vapor emission rate (MVER) requirements. These compulsory values are founded on laboratory testing rather than field experience, or worse still, on supposition rooted in unachievable and unrelated requisites. Repeatability problems and margin-of-error concerns must be taken into account for these widely specified thresholds. Case histories demonstrate the inaccuracy and unsubstantiated reliance on these commonly used test methods. Coating failures can occur even when we meet the MVER target value and successful coating performance can occur when the specified target values are not achieved. Recommendations that establish some practical criterion for deciding when, and where, not to coat concrete must be considered.

The most commonly specified quantitative moisture-related test standards for coating concrete are ASTM F1869 and ASTM F2170 (Fig. 1). These standards are routinely listed in product data sheets and specifications without any qualification as to the type of structure to be coated. Floor slabs on-grade within office buildings are treated exactly the same as below-grade wet wells in wastewater pump stations. For each test standard, manufacturers and engineers have generally adopted threshold values for deeming concrete suitable to be coated. For ASTM F1869, the target value is less than 3.0 lbs. of moisture per 1,000 square feet in 24 hours. This establishes an MVER. For ASTM F2170, the generally accepted maximum relative humidity (RH) for concrete is 80 percent. Scientifically derived data demonstrates that these threshold values are neither achievable nor realistic for below-grade structures.

APPLICABILITY

Specified RH minimums were selected based on laboratory testing under controlled conditions for indoor flooring installations. Acceptable MVER values were chosen because resilient sheet flooring manufacturers used an empirically established threshold. That 3.0 lbs. of moisture per 1,000 square feet per 24-hour target (or less) is acceptable for resilient sheet floor covering materials that are installed using adhesives on concrete floors. Coatings applied to concrete involve very different adhesion properties; yet the coatings industry has blindly accepted this threshold with no real supportive data.

The MVER test was developed specifically for bare concrete floor slabs. According to ASTM F1869, the calcium chloride test, as it is often called, should be conducted "under the same temperatures and humidity conditions expected during normal use" for the space to be coated. When this is not possible, the standard prescribes the following conditions: 75± 10 F and 50± 10-percent RH[1]. This means 65 F to 85 F and 40-to-60-percent RH. These are conditions that are maintained in buildings with climate control, not in below-grade pipe galleries, concrete channels or tanks. Most of the structures that require protective coatings in wastewater applications are at least partially, if not fully, below grade. In these structures, the concrete tends to assume the temperature of the ground, which is typically 50 F ± 5 F throughout North America. Additionally, many of these collection system or treatment plant structures are influenced by exposure to either static or fluctuating ground-water levels. Meeting those prescribed conditions requires the structure to be heated and ventilated. When heated, the soil side and the air side of the concrete will have very different RH levels. Furthermore, the normal use of these structures will involve immersion service or unheated and unventilated high humidity headspace environments. Neither scenario provides conditions similar to those under which the tests will be performed. It is clear that the 3 lbs. of moisture threshold value is not based on empirical coating adhesion, cure or performance data. And it is a fact that any one test result is a snapshot in time affected by conditions that are not likely under normal future exposure. Given this knowledge, the efficacy of the 3 lb. threshold is questionable in any application for coatings on concrete. Further, reliance on the applicability of the ASTM F1869 test method for below-grade structures is therefore dubious at best.

ASTM F2170 is entitled, "Determining Relative Humidity in Concrete Floor Slabs Using In-Situ Probes." Again, this test was developed specifically for concrete floor slabs and not concrete walls or overhead slabs. This standard states that the test shall be conducted at "the average ambient air temperature and RH typically found in *occupied spaces* during normal use."[2] These requirements make little sense for

below-grade concrete structures, which will rarely be occupied spaces. Therefore, neither temperature nor humidity will be normally controlled within these structures. Again in wastewater applications, most of these concrete spaces will be exposed to immersion or uncontrolled ambient air conditions during normal service. So the applicability of the RH test for determining if a below-grade concrete structure is ready to coat is suspect. For many coating manufacturers and specifying engineers, the generally accepted maximum RH value for coating concrete is 80 percent. This author has found this target almost impossible to hit in most subgrade structures.

REPEATABILITY AND MARGIN OF ERROR CONCERNS

The language in ASTM F1869 is crystal clear on the limitations of the test data. The MVER for a given test will "reflect the condition of the concrete floor at the time of the test and may not indicate future conditions."[3] By its very language, this test method presents repeatability challenges. For instance, the standard requires that the test site be at "the same temperature and humidity expected during normal use." This is almost always impossible to achieve in a below-grade structure during construction. If it involves a subgrade pipe gallery that will be heated during normal use, it will not be similarly heated during construction. If the structure to be coated is a buried wet well that will see immersion service normally, how does the calcium chloride test have any bearing on the effects of high or low MVER values?

This test method is designed to be used on floor slabs and there is no provision in the standard (no practical way) for performing this test on vertical surfaces or overhead surfaces. This renders the use of this standard difficult for anything other than a flooring application, at best.

As the standard correctly states, the movement of moisture vapor is "a natural process driven by environmental conditions"[4]. It is well known that large disparities in temperature and humidity across a concrete floor slab or wall (soil side to unburied side) drive MVER values. So if the RH of the soil on one side of a concrete wall and the RH of the tank side of the wall are both 100 percent, what will drive the MVER? And if the test is performed when the structure is heated and the test value is above the magic 3 lb. limit, won't the MVER drop once that structure is filled with water? Or if the heat is turned off and the test is repeated, will the result be repeatable? It probably will not be.

REAL-WORLD CASE HISTORIES

As an example, a below-grade influent channel used to convey wastewater flow to primary clarifiers needed to be coated. MVER test results were all greater than 3 lbs. per 1,000 square foot per 24 hours. This work was performed during a cooler time of the year, so the channel was being heated. The project schedule mandated that the coating work be completed and the channel be placed into service despite the failure to meet the specified 3-lb. MVER limit. Approximately two years later, the channel was dewatered and inspected. No blisters or other moisture-related deficiencies were observed. Figure 2 (p. 35) shows this coated channel at a wastewater treatment plant.

In another case, MVER values were measured on the wall surfaces of a new concrete pipe gallery. The gallery walls were common to coated tanks on either side which were in service at the time of the testing. The test values were all reported to be between 2.5-and-3.0 lbs. of moisture per 1,000 square feet per 24 hours. The gallery walls were prepared and coated. Within about one year, widespread liquid-filled blisters appeared in the gallery wall coatings (Fig. 3, p. 37). The pipe gallery was not heated, but it was ventilated. The ventilation was activated several months following the coating application work.

Much like ASTM F1869, the RH test method per ASTM F2170 has significant repeatability issues simply based on the expected change in conditions from construction exposure to normal use. This is because of both changing conditions and the presence or lack of moisture vapor retarders or external waterproofing on the positive (external) side of structures. As ASTM F2170 states, "moisture permeating from concrete floor slabs affects the performance of flooring systems such as resilient and textile floor coverings and coatings."[5] The amount of moisture vapor permeating from a concrete slab must be influenced by the amount of moisture vapor passing through the slab from the soil side or water side. Therefore, the lack or presence of a moisture vapor retarder or waterproofing coating on the exterior side of the concrete must make the RH test results vary substantially. Many existing below-grade concrete structures were not externally waterproofed. It is not typical for new buried structures to be waterproofed

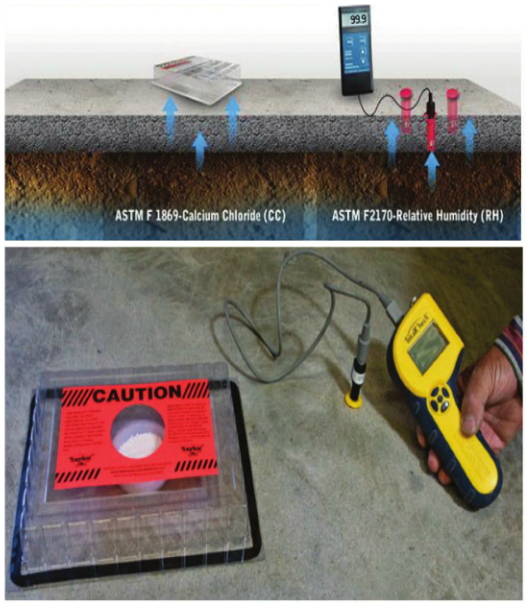

Fig. 1: These photos show the test set-up for both ASTM F1869 and ASTM F2170. All figures courtesy of the author.

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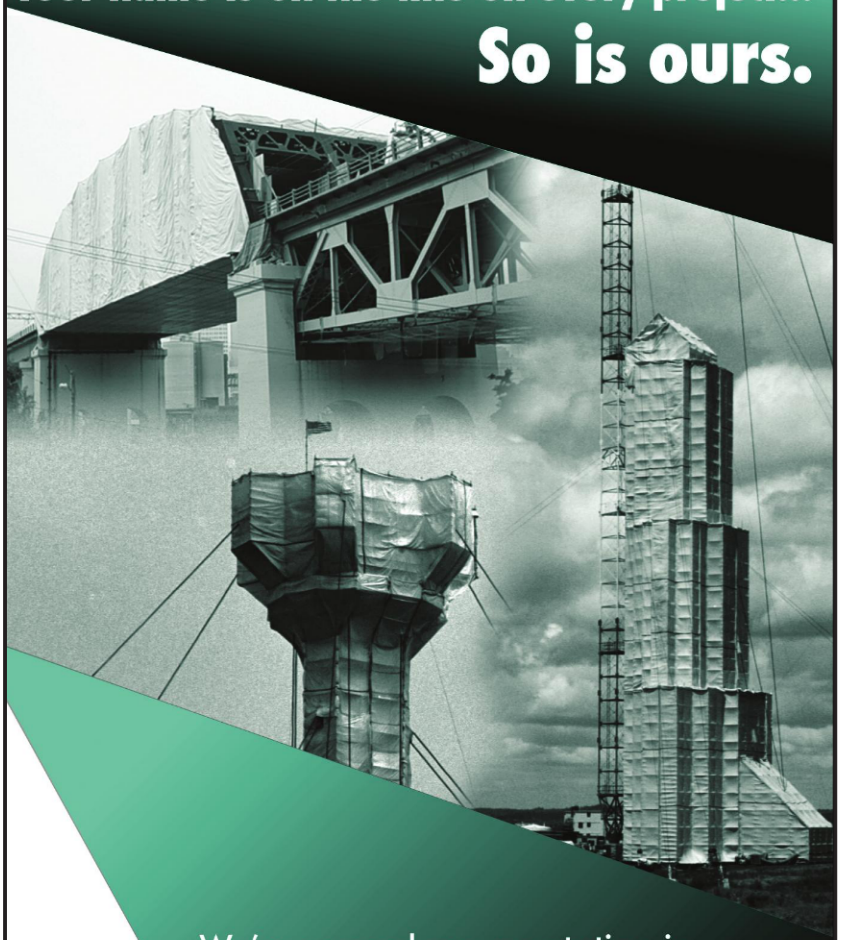

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unless there will be common spaces which are designed to be dry.

Whether Procedure A for hardened concrete or Procedure B for fresh concrete is utilized, ASTM F2170 requires that the probes be calibrated regularly using precise procedures. Improper calibration can significantly affect the test results. For either test procedure, the concrete floor slab is also required to be conditioned. The standard specifically requires that the "floor slabs shall be at service temperature and the occupied space above the floor slab shall be at service temperature and service relative humidity for at least 48 hours before making relative humidity measurements in the concrete slab.[6] Even if this equilibration exercise is accomplished, those conditions are unlikely to prevail under normal use in a below-grade structure. Once again, these facts point to repeatability and margin of error concerns for this test method, especially for below-grade structures.

Section 10.2 of ASTM F2170 presents how the depth for probe holes is determined using a table. That table differentiates between slab drying from one side (top only), or both sides (as in elevated slabs not in a metal deck) with 40- and 20-percent drill-depths, respectively. The standard then notes that, "testing at these depths will indicate the potential equilibrium relative humidity that will be established within the concrete slab after a low-permeability floor covering is applied." The basis for these determinations is not presented in the standard. Moreover, this statement makes the assumption that the moisture content in a slab is uniformly distributed and that simply isn't ever the case. This point again highlights margin of error concerns with this test method.

Coating manufactures and many engineers have adopted the threshold maximum value of 80-percent RH as a guideline for determining the coatability of concrete. This value is very difficult to achieve in new concrete structures that have been waterproofed, let alone for those that have not. It is also hard to reach RH values below 80

Fig. 2: An inspection of a below-grade channel two years after coating showed no blisters or other moisture-related deficiencies. Lining application of the epoxy glaze coat is shown on the left and the lining after completion on the right.

percent in existing structures for which the quality of waterproofing cannot be ascertained.

As an example, a new concrete wet well structure was constructed. Probe holes were formed at 40-percent depths in the wet well floor and lower wall elevations. The probes were inserted after the concrete cured for over 160 days. The concrete surface temperature and the air temperature and humidity conditions above the test probes in the structure were measured. The air was at 62 F with 50-percent RH. The surface temperature of the concrete was 54 F. The actual service conditions will be 100-percent RH (immersed or in the headspace over the liquid level) and the temperature will vary between about 55 and 75 F. As this project demonstrated, these tests can't be run at the actual service temperatures expected in most below-grade concrete structures. The RH measurements for the concrete were taken at the conditions reported above for two consecutive weeks and provided the following results (Table 1).

TABLE 1: RH MEASUREMENTS-CONCRETE WET WELL

Test Location	RH Values Week 1	RH Values Week 2
Floor - Test 1	90%	94%
Floor - Test 2	89%	81%
Wall - Test 1	92%	88%
Wall - Test 1	88%	85%

The coating work was delayed for another month on this project and still the RH threshold of 80 percent or less could not be achieved. The coating system was installed as the structure needed to be put into service. Inspection of the structure two-and-a-half years later showed no evidence of moisture-related coating failures (Fig. 4, p. 37).

Even in existing wastewater structures, reaching the 80-percent RH plateau has been problematic. For example, an existing concrete wet well needed to be resurfaced and coated. RH probes were installed at a 40-percent slab depth in accordance with ASTM F2170. Initial test values were all well above 85-percent RH. After nearly one month of heating and forced ventilation in the structure, the RH values were still above 80 percent. However, the trend in the data was consistently downward after four sets of measurements were taken at four locations. The decision to coat was made despite the higher than acceptable RH values specified. The coating work was carefully inspected after two years of service and no moisture-related coating problems were observed. Figure 5 (p. 39) shows a typical RH probe installation.

These real world case histories illustrate how the accuracy of this test method is influenced by too many variables for a fixed RH threshold value to be reliable.

CONSIDERATION OF QUALITATIVE MOISTURE TESTING

Experience has shown there to be significant merit to continued reliance on the use of the Plastic Sheet Test Method (ASTM D4263) for moisture testing when making the determination to coat concrete or not. Given that this qualitative standard does not require surface or ambient air conditions at the test site to be the same as the

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normal in-use conditions, it is a much more versatile test method than the quantitative methods previously discussed. However, this test can be misleading if not properly performed.

ASTM D4263, the "Standard Test Method for Indicating Moisture in Concrete by the Plastic Sheet Method," is used to indicate the presence of capillary moisture in concrete[7]. It is within its simplicity that one finds this standard's greatest value. It must be conducted when "the surface temperature and the ambient air conditions are within the established parameters for application of the coating system." These conditions can be controlled, so if moisture is present on the back of the polyethylene sheet, too much capillary moisture is present and the coating should not be applied. One can argue that many factors can influence the collection of moisture on the back of the sheet. The sheet must be sealed completely to the concrete at all edges. If air with a higher RH than the upper surface of the concrete gets under the sheet, moisture can collect on the backside of the sheet. But if this occurs, it is also likely that the minimum air and surface conditions within the structure are not within the coating manufacturer's established parameters for coating application. This reinforces the need to maintain those minimum requirements for temperature and RH for the 16 hours prescribed by the test method prior to examining the underside of the sheet and the concrete surface for the presence of moisture.

This test method does not detect high RH in the concrete. This is because under controlled coating-acceptable conditions, there is a lot of surface drying area around the plastic sheet patch. If there is a sustainable source of moisture on the soil side of a concrete floor slab or wall, and if the conditions which drive moisture vapor transfer are present, high MVER values are likely. The plastic sheet test is also not designed to indicate high moisture vapor emissions. However, if the minimum temperature and RH for a given coating system are provided within a structure, it is likely that those conditions will drive moisture vapor transfer through the concrete toward the warmer, less humid space. This means the moisture is more likely to collect on the plastic sheet if there is high vapor drive present.

Provided the plastic sheet test is carefully performed, experience has shown it to be effective when used alone or in concert with careful use of the quantitative test methods. For instance, when RH values (using ASTM F2170) have trended downward over time but not reached 80 percent, and

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Fig. 3: Within about one year after coating, widespread liquid-filled blisters appeared in the wall of a new concrete pipe gallery that had reported acceptable MVER test values.

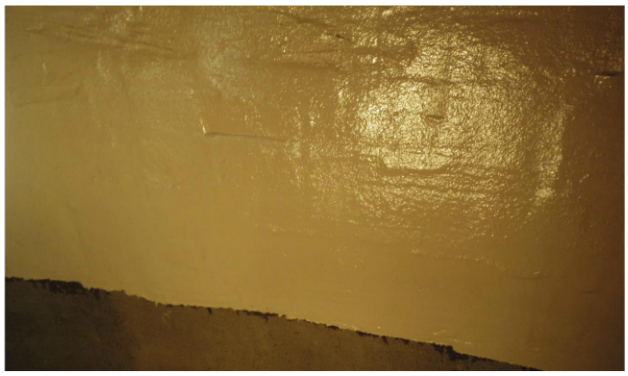

Fig. 4: Although acceptable testing conditions could not be met prior to coating a new concrete wet well, an inspection two-and-a-half years later revealed no moisture-related failures.

plastic sheet tests have indicated no presence of moisture, coating performance has been routinely successful in below-grade structures. Similarly where calcium chloride test results were above the 3 lbs. threshold level (as high as 6.0 lbs. per 1,000 square feet per 24 hours) and the plastic sheet test results were negative, coating application has been performed with no subsequent formation of moisture-related deficiencies in below-grade structures commonly filled with water or wastewater when in service.

PRACTICAL RECOMMENDATIONS

Based on the technical discussion provided and the experiences shared here, the following pragmatic recommendations are made when determining whether or not to coat concrete in below-grade structures.

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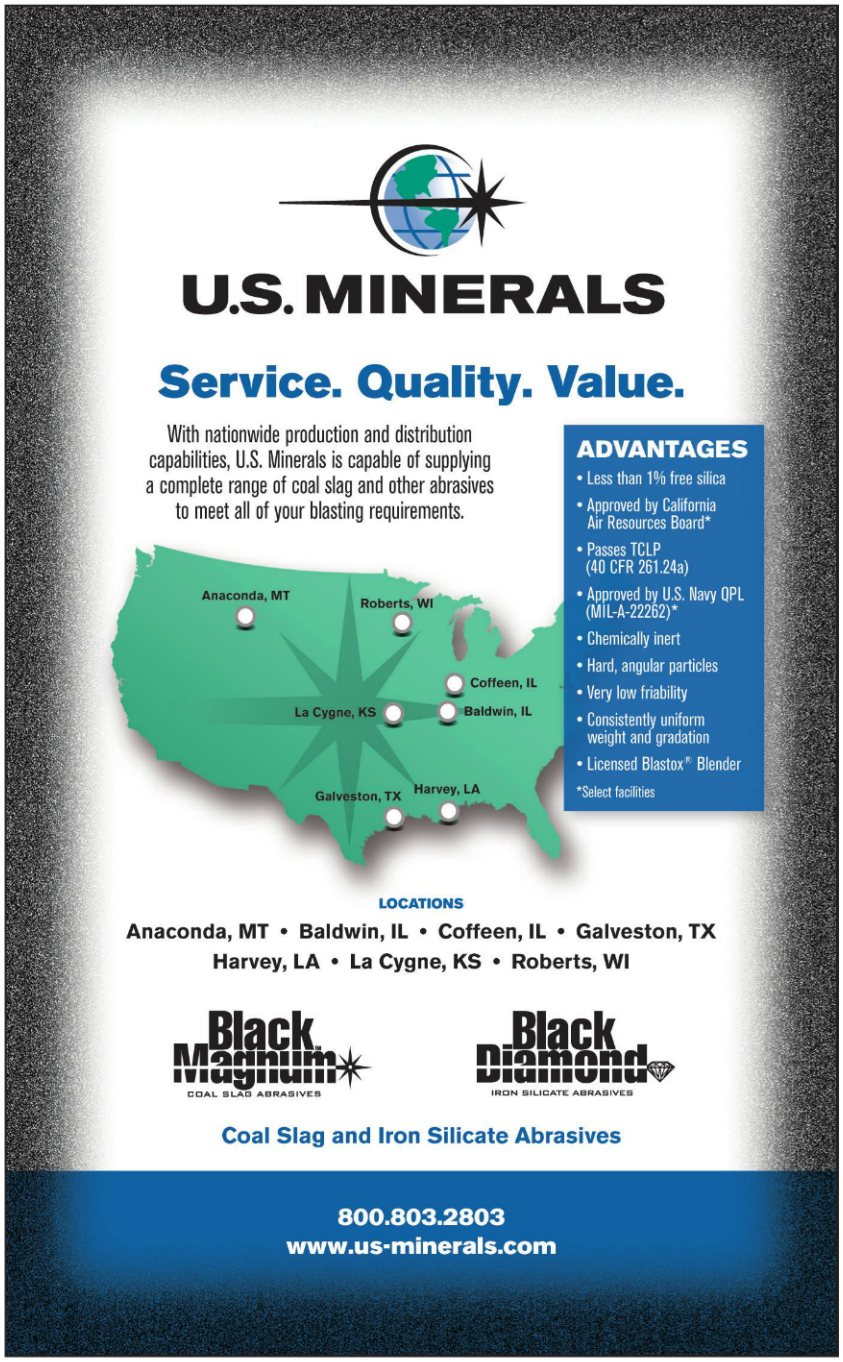
1. Only use the calcium chloride test for MVER testing for structures that will be heated or cooled and ventilated for personnel access in the future and will be waterproofed externally. This includes basements or pipe galleries or other spaces that will have common walls with soil or water/wastewater retaining structures. In such structures, the conditions found under normal use of the space can be provided, making the test results valid and useful. This test method is best suited for floor slabs. Where ASTM F1869 is utilized, consult with the selected coating manufacturer to determine the right MVER threshold. Several coating manufacturers have determined more realistic MVER levels for specific coating products that are above the arbitrary 3 lb. limit. If the structures were not or will not be externally waterproofed (when placed in service) or external waterproofing measures can't be affirmed (in the case of existing structures), do not use MVER testing in accordance with ASTM F1869. In these instances, rely on a combination of the plastic sheet test and the RH test (ASTM F2170). If the structure to be coated will normally be exposed to immersion service, rely on the plastic sheet test or the plastic sheet test performed in concert with RH testing where a downward trend in RH can be documented. Experience suggests that a maximum RH average range of 86-to-88 percent at mid-slab or wall depth for the probes and a negative plastic sheet test gives a successful result in most cases. The downward trend of the RH values must be based on three data points for at least four test sites within the structure over at least a three-week time period.

2. Recommend that external waterproofing be used (on the positive side) on all below-grade concrete structures to be coated on the negative side which will contain water or wastewater or will need to be dry and will be common to ground water or immersion exposure. This should be achieved with sheet moisture vapor retarders or barriers for floor slabs, and with either sheet waterproofing or liquid-applied coatings for walls. This recommendation should find its way into specifications whenever and wherever possible.

3. In below-grade wastewater structures, do not recommend coating the concrete more than a few feet below the normal operating level. Municipal wastewater is generally not corrosive to concrete or reinforcing steel for immersion exposure unless chloride concentrations in the wastewater are too high. This also provides additional unsealed concrete surfaces through which moisture vapor can escape.

4. If the average ambient air temperature and RH typically expected in the spaces of a structure during normal use cannot be achieved during the test timeframe (including conditioning of the concrete to the



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service temperature), there is no point in attempting to test for concrete RH in accordance with ASTM F2170. Under such circumstances, the plastic sheet test should be relied upon provided the following qualifications are met.

A. Do not coat concrete that has not been externally waterproofed where high or fluctuating ground-water levels are known to be present relative to the structure's burial depth.

B. If the headspaces (under normal service conditions) need to be coated, but not the lower walls and floor of the structure, reliance on the plastic sheet test results will be acceptable provided the external surfaces of the headspaces have been waterproofed.

5. For a new concrete structure that is known to be externally waterproofed and in which conditions can be controlled to meet the normal use temperature and RH, a downward trend in RH values should be considered more important than the target RH value. This would include the requirement for the 48-hour conditioning of the concrete temperature. Under these conditions, it is recommended that a downward trend in RH values in concert with a negative plastic sheet test result would be acceptable for coating concrete. The recommended declining trend would have the following guidelines.

Fig. 5: This photo shows a typical RH probe installation.

A. A maximum average RH value range of 86-to-88 percent based on at least four test sites.

B. A downward trend with a consecutive decline in RH values over three data points in three weeks for a minimum of four test sites per structure.

The recommendations made here are not meant to apply universally to all below-grade concrete structures. The actual field conditions during construction and the expected in-service exposures must be carefully considered on a case-by-case basis. But it is this author's hope that the information presented will be helpful to the coatings industry from a guideline perspective.

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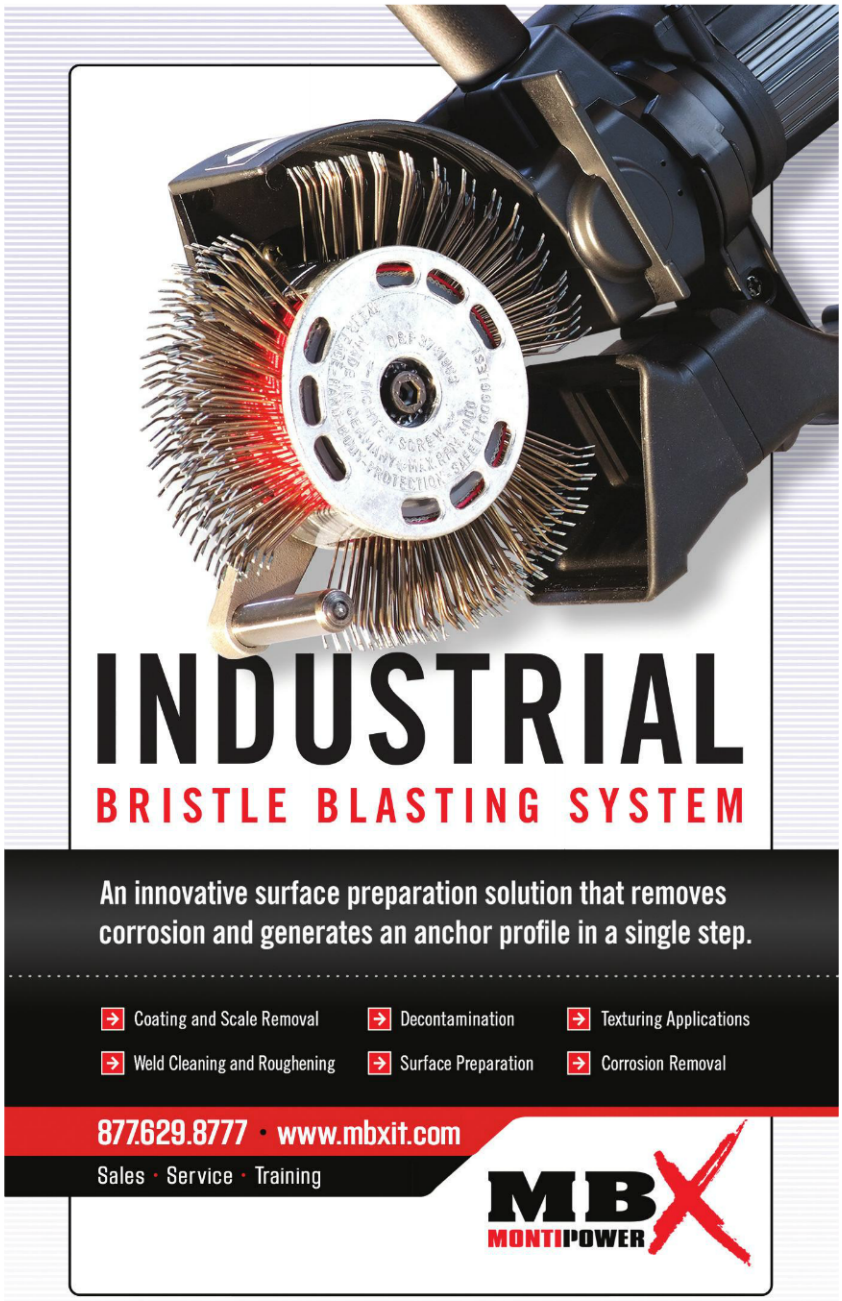

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SPCO Holdings Pte. Ltd. 10 Tuas Ave. 6 Singapore, Singapore 629586 65-9847-3403; rogercsw@singnet.com.sg			•	•			•										
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Prothane Industrial PO Box 2838, Klerksdorp, South Africa 2570 Gert: 27-018-4841181; info@prothane.com							•	•	•	•					•		
Sharpshell Industrial Solutions 33 Kelly Road Jet Park, Johannesburg, South Africa 1461 Sales: 27-71-344-4699; sales@sharpshell.co.za	•	•	•	•	•	•	•	•	•	•				•	•	•	•
TMS Group Industrial Services (Pty) Ltd. Unit 11A, 2 Tonetti St., Midrand, South Africa 1685 David Madisa: 27-11554-8600; dmadisa@tmsg.co.za	•	•	•	•		•	•	•	•	•			•	•	•		•

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Hyundai Samho Heavy Industries Co., Ltd. 1700 Yongdang-ri, Samho-eup, Yeongam-Gun, Jeollanam-Do, South Korea 526-701; 82-52-202-2114; hhiopr@hhi.co.kr				•													
Samsung Heavy Industries 530, Jangpyeong-Ri, Sinhyeon-Eup, Geoje-Si South Korea; Jeong Jinhyung: 82-55-631-9472	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•

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Optimiza Protective & Consulting, SL. Calle Enrique Villar Nº13 4ºA, Murcia, Spain 30008 34-630850927; optimiza@optimizaconsulting.es	•	•	•	•	•	•	•	•	•	•							
Proinbel Plata 24-26 Poligono San Cristobal Valladolid, Spain 47012; Eduardo Beltran: 34-98-339-5644 proinbel@proinbel.com	•			•	•	•		•	•	•		•	•	•	•		
SA Julio Crespo Poligono Ind Albresa Ave., Madrid 16, Valdemoro Spain 28340; Julio Crespo Villalobos: 34-6-77939323	•	•	•	•	•	•	•		•			•		•	•	•	

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Blasting Group Service Ltd. 15/5 Moo 1 Tambol Rahang, Amphur Lardlumkaew, Parthumthani, Thailand 12140 Chaiyan Dejtisak: 66-1-6259128; cdejtisak@hotmail.com	•	•	•	•	•			•	•	•	•			•	•	•	
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K.D.W. Company Limited 104/34 Liab-Klong-Song Rd. Bangchan Klong-Samwa, Bangkok, Thailand 10510 Chaiwat: 66-2-9144548; gm@kdwworld.com			•	•				•	•	•	•			•	•		
Mobicoat Ltd. 130/12 Moo2 Surasak Rd. Sriracha Chonburi, Thailand 20110; Nitch Yamnoi 66-85-721-6687; services@mobicoat.com	•	•	•	•	•	•	•	•	•								
Trepax Innovation Co., Ltd. 76-78 Moo 13, Suksawat Rd., Samutprakarn, Thailand 10130; Paul Greenhalgh 66-2818-0544; paul@trepax.co.th			•	•			•	•	•	•	•				•		
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Theo Richards General Contractors Ltd. E. Street, Fanny Village, Point Fortin, Trinidad; Theophilus Richards 868-365-0310; itor1@yahoo.com			•				•										•

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Protech Endustryel Teknik Koruma Ltd. Perpa Ticaret Merkezi B Blok Kat:11 NO: 1659 Okeydani Sisli, Istanbul, Turkey 34384; 90-532-405-8511; info@pro-techtr.com	•	•	•	•	•	•	•	•	•	•				•	•		
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Robert Ikenberry: 925-461-1500; rikenberry@cecmain.com

Capital Painting
6155 Stockton Blvd., #6, Sacramento, CA 95824
George Zettas: 916-837-7091; info@capital-painting.com

CAWC Waterproofing & Restoration
800 Burlway Rd., Burlingame, CA 94010; Roy Mayer
650-347-1404; roy@cawcwaterproofing.com

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Jon Claypool: 562-285-6520; jclaypool@clcoatings.us

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Tom Reese: 714-345-2611; treese1074@aol.com

Commercial Sandblast
2678 E. 26th Street, Vernon, CA 90058; Mark Quesada
323-587-1256; contactcsc@commercialsandblast.com

Cor-Ray Painting 10114 Shoemaker Avenue, Santa Fe Springs, CA 90670; Christopher Goutagny: 629-069-7702
cgoutagny@corraypainting.com

Crest Coating, Inc.
1361 S. Allec St., Anaheim, CA 92805; Jeff Erickson
714-563-7305; jeff@crestcoating.com

Demac Construction Services, Inc.
25379 Wayne Mills Pl., # 241, Valencia, CA 91355
David McGraw; 661-257-6866; demac_24@yahoo.com

Dunkin & Bush, Inc.
6837 Krebs Rd., Bakersfield, CA 93308; Mark Rondeau
503-285-0100; mrondeau@dunkinandbush.com

EW Corporation
1002 East Main St., El Centro, CA 92243; Karla Lopez
760-337-0020; lopezk@ewcorporation.com

Goodwest Rubber Linings
8814 Industrial Ln., Rancho Cucamonga, CA 91730
Patrick Sears: 909-758-4725; psears@goodwestlining.com

Green's Industrial Painting, Inc.
3045 E. Inland Empire Blvd., Ontario, CA 91764
Don Green; 800-747-1553; dongreen9226@aol.com

Hallmark Painting Services, Inc.
11242 Woodrush Crt., San Diego, CA 92128
Gul Erel: 805-899-4541; hallmarkptng@gmail.com

HiTech Painting
431 W. Douglas Ave., El Cajon, CA 92020
Mark Heuslein; 619-456-9007; mark@hitechpaint.com

HPC, Inc.
413 Dawson Drive, Camarillo, CA 93012
Jeff Glass: 805-383-2868; jeffg@hydropressure.com

Industrial Coatings & Fireproofing
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Jim Monzon: 562-426-7105; jmonzon@nacoatings.com

Innovative Painting & Waterproofing, Inc.
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Don Dancey: 714-257-0200; don@911waterproofing.com

International Waterproofing
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Stephen Sankowich: 415-720-1154; iwp1@aol.com

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PO Box 3183, Auburn, CA 95604; Chris Harris
530-888-0580; cjh@jlhpainting.com

Jeffco Painting & Coating, Inc.
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707-562-1900; matt@jeffcoptg.com

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415-454-1500; tim@jtspainting.com

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John "Jay" Koval: 650-642-7781; jay@krsurface.com

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Louis Peick; 909-641-1573; lppaint@verizon.net

Larry Bashor Sandblasting & Protective Coating
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661-588-4737; lsbashor@netzero.com

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14952 Wilson Street, Midway City, CA 92655
Bruce Cossell: 714-892-3536; sales@lyncoassociates.com

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510-979-0420; reka@magnumdrywall.com

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Andy Sterling: 760-330-3012; asterling@mobilepipe.net

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Greg: 800-942-3166; info@modernmasters.com

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Steve Murphy: 562-427-7720; steve@murphy.ac

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Tom Unsell: 951-471-3388; stephen@nc-lc.com

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Bruce Williams: 619-232-3200; bwilliams@pacship.com

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Patrick Ricciardi; 951-788-2070; pat@ptitan.com

Painting America Corporation 6475 E. Pacific Coast Highway #406, Long Beach, CA 90803; Rick Carter
866-254-7700; rcarter@paintingamericacorporation.com

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Jacqueline Parada: 858-748-3540; paradaj@cox.net

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Jerry Watterson: 818-704-9940; jerry@premierptg.com

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bweatherford@qedsysinc.com

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Allen Tabrizi: 714-377-1427; info@raiderpainting.com

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Colorado Springs, CO 80905; Rick Bingjman
719-635-8626; linex.coloradosprings@gmail.com

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Mark Bemus: 303-431-8445; mabemus@lbtdsl.com

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Ray Desai: 863-398-4344; americoatusa@yahoo.com
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727-504-4480; bridgemasters@gmail.com
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6525 Greenland Road, Jacksonville, FL 32258
Josh Tankersley: 904-268-4559; josht@corrocoat.com
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Dependable Painting and Wallcovering, Inc. 14290 SW 97th Ave., Miami, FL 33176; Steven Schneider 305-233-3830; dependable@sprintmail.com			•		•	•											
Diiorio Property Svc, Inc. 3105 Bridgestone Dr., Jacksonville, FL 32216 Roger: 904-419-4880; diiorioproperty@aol.com	•										•			•			•
Diversified Container 8831 Moncrief-Dinsmore Road, Jacksonville, FL 32219 Roy Alexander: 904-613-7066; roy@divconjax.com	•		•						•								•
Diversified Painting & Maintenance, Inc. 3248 Sawgrass Creek Cir., Kissimmee, FL 34772 Alan: 321-377-2987; dmsadailey@earthlink.net		•	•		•	•	•	•	•								
DS Industries, Inc. 2195 N. Andrews Ave. Ext., Ste. 10 Pompano Beach, FL 33069; Dan Shields: 954-973-1700 dsindustries@aol.com		•		•				•	•	•				•	•		
Enviro Painting Corp. 1323 Riverside, Tarpon Springs, FL 34689 Emmanuel Kavouklis: 727-741-8088; flsandblast@aol.com	•		•					•	•	•	•	•	•		•		
EPI Essential Painting, Inc. 4445 SW 35th Ter., Ste. 120 Gainesville, FL 32608; Andrew Schneider: 386-623-6066 andrew@epipainting.com			•			•	•		•	•							
Exceletech Coating & Applications, LLC 901 12th St., Clermont, FL 34711; Larry Muzia 352-394-2155; lm@excelcoatings.com					•				•	•	•				•		
Faux Creation Studios 6773 Boca Pines Trl., Apt. A Boca Raton, FL 33433; Mike Whitehead: 561-722-2166 mike@fauxcreationstudios.com																	
Finishing Systems of Florida, Inc. 160 Dog Track Road, Longwood, FL 32750 Brenda Soper: 407-834-6280; sales@finishingsystems.us	•																
G and G Services 23060 Tinas Lane, Fort Myers, FL 33913; Guadalupe 239-313-0206; gandgblastingservices@gmail.com			•				•		•	•					•		
Gemini, Inc. 4871 Blue Jay Circle Palm Harbor, FL 34683; Michael Monokandilos 727-938-6922; mgeminik@aol.com	•							•		•	•	•					
Gemstone, LLC 1010 Kennedy Drive, Suite 406 Key West, FL 33040; Cameron Jewell: 305-294-3233 cameronrjewell@gmail.com		•	•	•							•	•	•				
Gulf Coast Contracting, LLC PO Box 2178, Tarpon Springs, FL 34688 Manuel Gialousis: 727-938-6081; gulfcoastoffl@aol.com	•									•	•	•	•				
H.I.S. Painting, Inc. 8680 Schirra Ct., Titusville, FL 32780; Dave Heyne 321-567-2468; daveheyne@hispaintinginc.com			•	•		•		•	•	•	•	•	•	•	•		•
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Jack Allen, Inc. 2941 Southwest 1st Terrace, Ft. Lauderdale, FL 33315 Kelly Allen; 954-522-3044								•			•						
John The Greek Painting 970 NE 40th Ct., Oakland Park, FL 33334; John/Kirk/Tony 954-563-4325; tony@johnthegreekpainting.com												•		•			
K2 Industrial Services, Inc. (dba Mansfield Industrial, Inc.) 1325 W. Detroit Blvd, Pensacola, FL 32534; Jimmie Potts 850-477-6437; jimmiepotts@k2industrial.com	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•
KempCo Painting Contractor, Inc. 1830 Vista Way Margate, FL 33063; Demetrios Kekempanos 561-313-4502; demetrios_kempco@mac.com																	
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M & J Construction Company Of Pinellas County, Inc. 809 S. Safford Ave., Tarpon Springs, FL 34689; James 727-938-6478; mjconstruction@mjconstruction.net	•		•								•	•			•		
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Mansfield Industrial, Inc. 1325 W. Detroit Blvd., Pensacola, FL 32534; Hank Barron 251-653-1239; hankbarron@k2industrial.com		•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•
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Master Blaster 5015 Saint Lucie Blvd., Fort Pierce, FL 34946; Arturo Silva 772-460-2487; masterblasterfl@hotmail.com		•	•	•		•	•	•	•	•							
Matrix Coatings Corp. 3575 Investment Ln., West Palm Beach, FL 33404 Larry Slaon: 561-848-1288; Larry@matrixcoat.com		•	•	•	•		•							•			
MJM Construction, LLC 643 Bayshore Drive, Tarpon Springs, FL 34689; Katerina Makryllos: 727-934-6846 mjm_construction@verizon.net	•									•	•	•	•				
Monoko, LLC 1037 Peninsula Ave., Tarpon Springs, FL 34689 Keri Monokandilos: 727-940-3244; monokollc@aol.com	•			•		•		•	•	•	•	•	•				
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Overspray Removal Specialists 7415 Weeping Willow Drive, Sarasota, FL 34241 George Colliard: 800-835-5858; george@overspray.com	•	•	•	•	•	•	•	•	•	•				•			
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Professional Tank Cleaning 3160 NE 3rd Ave., Oakland Park, FL 33334 Chad Anschuetz: 954-533-1135; chad@protank.net	•	•		•	•	•		•	•	•	•		•	•	•	•	•
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QinetiQ North America Mailstop: ESC - 23, Kennedy Space Center, FL 32899 Anthony Cengia: 321-861-5334																	
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S&D Industrial Painting 1575 Rainville Rd., Tarpon Springs, FL 34689 Irene Tirikos: 727-937-2080; sddamalos@aol.com			•									•	•			•	•
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Service Painting Corporation 1423 West Long St., Orlando, FL 32805 Todd Monroe: 407-872-7555; tmonroespc@aol.com				•		•				•	•	•				•	•
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Southern Painting & Blasting, LLC 175 Anchor Road Casselberry, FL 32707; Larry Stempkowski 407-822-1760; lstemp@southern-paint.com			•		•		•	•	•	•				•	•	•	
Southland Painting Corp. 2635 NW 4th St., Ft. Lauderdale, FL 33311 Jef Verhelst: 954-748-2770; liz@southlandpainting.com			•							•	•	•				•	•

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Sumbry Solutions & Services, LLC
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Angel M. Pareja: 305-234-8355; info@tcsempire.com

Total Containment Solutions, Inc.
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Michael D. Whitener; 239-275-6235; mike@tcsliners.com

Turner Coatings, Inc.
1865 Industrial Park Rd., Mulberry, FL 33860
Ron Taylor: 863-425-4563; turner.coatings@verizon.net

United Decorating Services, Inc.
14618 Dara Ln., Spring Hill, FL 34610; David Lucariello
727-481-7830; davidlucariello@yahoo.com

Universal Painting Corporation 3810 Drane Field Road
Suite 3, Lakeland, FL 33811; John Aldrich: 863-686-2320
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Vanguard Vinyl Coatings, Inc.
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Roy Jensen: 888-282-4777; inc5546@bellsouth.net

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MIke Lynch: 813-715-7117

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Alaga Paint Co. Inc.
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All-Safe Industrial Services, Inc.
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Alpha Insulation & Waterproofing
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American Railcar Industries
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Brand Energy & Infrastructure Services
1325 Cobb International Dr., Ste. A-1, Kennesaw, GA 30152
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South GA Tower & Tank, Inc.
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Toole Floor Coating Co.
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Visuron Technologies
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CL Coatings, LLC
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CR2 Painting Ltd. 3950 W. Gardner Rd., Verona, IL 60479; Kurt Williams 815-302-3510; kurt@cr2painting.com	•	•					•	•	•	•	•	•	•	•	•		•
Crown Painting, Inc. 10124 187th Street, Mokena, IL 60448; Eve Conversa 708-478-0505; eveconversa@crownpainting.net	•	•					•	•	•	•	•	•	•	•	•		•
DBM Services, Inc. 9850 W. 190th St. Ste. A, Mokena, IL 60448 Mike Lynch: 708-995-1473	•	•							•	•	•				•		
Demos Painting & Decorating, Inc. 1730 W. Belmont Ave., Chicago, IL 60657 Mike Demos: 773-248-5364	•										•						
Dragoo Painting, Inc. PO Box 722, Peoria, IL 61652 Dennis Dragoo: 309-686-0782														•			
Eagle Painting & Maintenance Co. 1865 Bernice Road, Lansing, IL 60438; Steve Sakoutis 708-862-8712; eagle_paul@sbcglobal.net	•	•	•		•	•	•		•	•	•	•	•	•	•		
Georgia and Company, Inc. 875 N.Michigan Ave. Floor 31 Chicago, IL 60611; Jay Mortensen: 866-252-7285 customerservice@georgiaandco.com	•		•						•	•	•	•	•	•	•	•	•
Giant Maintenance & Restoration, Inc. 300 N. Seymour, Suite D, Mundelein, IL 60060 Mark Giarrante: 847-388-3711; Info@giant2u.com										•							
Hanfland Painting Contractors 105 E. Louis, Sigel, IL 62462; Gerald Hanfland 217-844-3322; handflandcontrs@consolidated.net		•					•	•	•	•		•		•	•		
Holger Hanson Decorating 6327 N. Pulaski Rd., Chicago, IL 60646 773-478-8448; whh5122@aol.com									•	•	•	•					
Interstate Coatings and Contractors Ltd. 10013 S. Ave. N, Chicago, IL 60617; Carlos 773-425-9979; icc.carlos@hotmail.com	•	•	•	•	•	•	•	•	•	•	•	•		•	•		
Jetco Ltd. PO Box 908, Lake Zurich, IL 60047; Ken Brend 847-438-4550; stevebrendjr@comcast.net									•	•		•		•	•		
Lankford Construction 1455 Karlens Way, Johnsburg, IL 60051; Mark Zamiar 708-774-5838; mzamiar@lcco.com			•	•	•		•	•	•	•	•	•	•	•	•	•	•
Marley Industrial Services 21329 N. 11th Ave., Fillmore, IL 62032; Paul Marley 217-710-1840; marleyindustrialserv@gmail.com		•					•		•	•	•	•		•		•	
Maxcor Incorporated 900 Country Creek Drive, New Lenox, IL 60451 John Sullivan, Jr.: 815-838-4370; Info@maxcor-inc.com										•	•						
McCahill Painting Company 324 Rocbaar Drive, Romeoville, IL 60446; Tim McCahill 888-886-5544; info@mccahillpainting.com							•			•		•			•		
Metro Coatings 2llc PO Box 2411, Decatur, IL 62524; James K. Burnett 217-972-7450; metro972@earthlink.net			•				•		•	•	•	•		•			
Moore Painting Co. 4522 Alton Commerce Parkway, Alton, IL 62002 Dennis Moore: 618-465-0118			•				•		•	•	•	•					
Morrissey Contracting Company, Inc. 703 Southmoor Pl. Godfrey, IL 62035; Barbara Morrissey McGrew 618-466-9006; mccpainter@morrisseycontracting.com			•				•		•	•				•		•	
Ostrom Painting & Sandblasting, Inc. 1110-8th Avenue PO Box 4120, Rock Island, IL 61204; Robert Walker 309-788-1933; rwalker@ostrompainting.com			•				•			•	•	•	•	•	•	•	
Paint Platoon USA 7307 Swan Way, Cary, IL 60013; Scott Kuperman 847-639-8800; contact@paintplatoon.com			•	•	•		•	•	•	•	•						
Painters USA, Inc. 1415 Centre Circle, Downers Grove, IL 60515; Craig Nichols 800-999-8715; estimate@paintersusainc.com			•		•		•	•	•	•	•	•		•	•		•
Paint-Right Painting, Inc. 153 Waxwing, Naperville, IL 60565; Kathy Wright 630-961-9249; prpwright@msn.com			•						•	•							
Pecover Decorating Services, Inc. 1033 Oliver Avenue, Aurora, IL 60506; William Nielsen 630-892-9100; info@pecover.com			•		•		•		•	•	•				•		•
Prater Paint and Waterproofing Avenue G Island/PO Box 13, Sterling, IL 61081 Wallace Prater: 815-626-5165; prater@sbmconnect.com										•	•	•	•		•	•	
Precision Applied Coatings 15 Production Pkwy., Cahokia, IL 62206; Gary Van Pelt 618-337-2581; glvp229@msn.com			•	•	•	•	•	•	•	•	•		•	•	•	•	•
Quality Assured Industrial Coatings 804 Hopp Hollow, Alton, IL 62002; Beth Johnes 618-466-0003; tjqaic@charter.net			•	•		•	•	•	•	•	•	•	•	•	•		
Quincy Industrial Painting Co. 2000 N. 24th St., Quincy, IL 62301; Curt A. Hilbing 217-222-6710; curth@quincyindustrial.com			•	•		•	•	•	•	•	•	•	•	•	•	•	•
Red Feather Painting, Co 711 Becker Road, Glenview, IL 60025; John Sochacki 847-724-4936; info@redfeatherpainting.com			•	•			•	•	•	•	•	•			•	•	•
Robert W. Britz Painting Co., Inc. 14256 Frazee Road, Divernon, IL 62530; Jamie L. Weber 217-628-3511; britzpainting@mwii.net			•								•		•				
Royal Line Resins, Inc. 1107 Carolina Drive Drive, West Chicago, IL 60185 Al Guiliano: 847-678-3922; paison53@aol.com												•		•	•	•	
SCS Services, Inc. 4201 W. Kinzie St., Chicago, IL 60624; W.P. Ward 773-533-1061; wayward@bigplanet.com			•		•		•		•	•	•	•					
SEI Coatings, LLC 8450 W. 191st St., Unit 19, Mokena, IL 60448 Jim Sourbis Jr.: 815-464-3053; ssourbis@clcoatings.us			•									•					
Shopwerks, Inc. PO Box 47, Gardner, IL 60424; Darren Sorensen 815-237-8000; ds@shopwerksinc.com			•	•	•	•	•	•	•	•	•				•	•	
Shopwerks, Inc. 3014 South Division, Braceville, IL 60447 Darren Sorensen; 815-237-8000			•	•	•	•	•	•	•	•	•				•	•	
Spectrum Contracting Corporation 1411 W. Bernard Dr., Addison, IL 60101; Tony Lee 630-916-6800; tlee@spectrumcontracting.com			•	•		•	•		•	•	•			•	•	•	
Spray Specialists, Inc. 30353 Dowell Rd., Mchenry IL 60051; Bob Wargaski 847-878-2867; spray1@oawl.net			•									•					
Tecorp, Inc. 2221 Muriel Ct., Joilet, IL 60433; Nick Visvardis 815-726-9192; nvisvardis@tecorp.us			•	•		•		•	•	•	•	•			•		•
Triangle Decorating Company, LLC 710 Remington Road, Schaumberg, IL 60173 Scott Hall; 847-490-6600												•		•			

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Neil F. Finn Jr.: 312-431-5010; finn@utlx.com

United States Corrosion Engineers
800 Railroad St., Joliet, IL 60436; Tom Gajcak
815-727-7785; uscorreng@ameritech.net

Versatile Painting, Inc.
RR#2 Box323A, Ramsey, IL 62080
David T. Marley Sr.: 618-423-9219

W. F. Scott Decorating
2201 3rd Ave., Rock Island, IL 61201; Donald Wentler
309-786-1221; don@scottdecorating.com

Walker's Sandblasting & Painting
1066 W. South St., Girard, IL 62640; Jim Walker
217-652-9049; jnswalker2003@yahoo.com

Ziegler Industries, Inc. 1247 E. County Rd. 2300
Dallas City, IL 62330; Gregory Malinski: 319-470-4285
service@ziegler-industries.com

INDIANA

ACS Ventures, LLC dba American Clean & Seal
4469 Bragdon St., Indianapolis, IN 46226
Jeff Wood: 317-536-0211; jwood@acs-indy.com

Advanced Painting & Sandblasting
312 N. 8th St., Decatur, IN 46733; Joe Friedt
advpainting@live.com

Al Porter Commercial Painting, Inc.
1402 Sherwood Dr., Greenfield, IN 46140
Al Porter: 239-280-8606; sporter206@gmail.com

American Clean & Seal
4469 Bragdon St., Indianapolis, IN 46226
Mark Anderson: 317-536-0211; info@acs-indy.com

Busy Bee Painters, Inc.
2315 Wayne Trace, Fort Wayne, IN 46803; Gary Lambroff
260-422-9390; busybpaintersinc@aol.com

Cannon Sline Industrial, Inc.
4527 Columbia Avenue, Hammond, IN 46327; Ken Jones
219-937-8100; kenjones@k2industrial.com

Civil Coatings and Construction, Inc.
1651 West Lincolnway, Valparaiso, IN 46385
John S. Tsahas: 219-531-5300; h9kwak@aol.com

Concrete Tune-Up
2600 S. Henderson Street, Bloomington, IN 47401
Mark: 812-325-1633; concretetuneup@outlook.com

Connor Fine Painting, Inc.
1431 S. Meridian Street, Indianapolis, IN 46225
Michael Bura: 317-853-7919; mikeb@connorpainting.com

Eagle Services Corporation 2702 Beech Street
Valparaiso IN 46383; John Augustinovich: 219-464-8888
jaugie@eagleservices.com

Fresh Colors Painting, LLC
5153 Commerce Square Dr. Suite B, Indianapolis, IN 46237
Bryan Tibbetts: 317-783-0337; bryan@freshcolors.com

Funkhouser Finishes, LLC
2893 S. 775 W., Morgantown, IN 46160; Kevin Funkhouser
317-847-4537; kevin@funkhouserrestoration.com

K2 Industrial Services, Inc., Cannon Sline Industrial Services, Inc. Division 4527 Columbia Ave. Fl. 2, Hammond, IN 46327; 219-937-8132; kenjones@k2industrial.com

Midwest Pipe Coating, Inc.
925 Kennedy Avenue, Schererville, IN 46375
Jackie Martin: 219-322-4564; martinj@midwest-pc.com

N.I. Spanos Painting, Inc.
PO Box 10908, Merriville, IN 46411; John Spanos
219-662-9092; daliaspanos@comcast.net

N.A. Logan, Inc. 3000 Burr St.
Gary, IN 46406; Anthony (Tony) Logan
219-989-0700; tlogan@nalogan.com

N.A. LOGAN is a shop and field industrial painting contractor & applicator of specialized coatings, fireproofing materials and polyureas as part of new construction and maintenance projects. Servicing facility owners and their general contractors since 1957 in areas surrounding south Lake Michigan. Surface preparation (an array of media) and lead abatement is also performed. www.nalogan.com

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765-288-3611; pmahlock@odleinc.com

Panther Industrial Painting, LLC
15790 Springmill Drive, Mishawaka, IN 46545
Mary Jo Tirikos: 330-755-6031; mjtirikos@msn.com

Preferred, Inc.- Fort Wayne 6021 Highview Drive
Fort Wayne, IN 46818; Philip Wilsford: 260-483-8383
pwilsford@preferredinc.com

Prism Painting Company, Inc.
PO Box 1944, Highland, IN 46322; Jim Mitchell Jr.
219-322-4800; jimjr@prismpainting.com

Robinson Painting and Acoustical Company, Inc.
4117 North Saint Joseph Ave., Evansville, IN 47720
Dan Robinson: 812-423-8832; info@robinsonpainting.com

Shamrock Painting
1311 Elm Street, Valparaiso, IN 46383; John Harrahill
219-462-7776; shamrockpaints@gmail.com

Shelby Coatings, Inc.
4575 West Range Rd., Shelbyville, IN 46176
Garrick A. Ballard: 317-850-0155; amigarrick@aol.com

Shield Industrial Painting Company PO Box 1100
Greenfield, IN 46140; Andrew Robertson
317-847-1364; andrew@shieldpainting.com

Simms Painting Co., Inc. 1600 S. New York Ave.
Evansville, IN 47714; Brent van Zandt: 812-428-6900
simmspainting@sbcglobal.net

Specialty Construction Services
2915 Bristlecone Dr., Schererville, IN 46375;
John S. Tsahas: 219-531-5300; jtsahas@aol.com

Standard Coating Service, Inc.
511 East Troy Avenue, Indianapolis, IN 46203; A.K. Das
317-331-8826; standcoat@sbcglobal.net

Summit Painting Co., Inc.
6251 Maplecrest Rd., Fort Wayne, IN 46835
Ed: 260-432-1549

Three Star Painting
2880 E. 84th Pl., Merrillville, IN 46410; Gus Tsahas
219-947-3021; spongis13@aol.com

W. Kendall & Sons, Inc.
10270 W. U.S. Hwy. 30, P.O. Box #9, Wanatah, IN 46390
Gary Rice: 219-733-2412; wkendallsons@frontier.com

Ziolkowski Construction, Inc. Painting Division
4050 Ralph Jones Drive, South Bend, IN 46628
Mike Hamilton: 574-287-1811

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Allen Blasting and Coating, Inc. 814 East Adams Street
New London, IA,52645; Chris Andrews: 319-217-6314
candrews@allenblastingandcoating.com

Allied Construction / Colors, Inc.
214 35th St., Bettendorf, IA 52722
Don Pruitt: 563-355-2651

Arndt Enterprises, Inc.
2557 195th St., Dewitt, IA 52742
Michael LeBlanc: 563-659-2062

Central Tank Coatings
22528 Canoe Rd., Elgin, IA 52141; Kelly Koehn
563-380-2647; ctcinc@alpinecom.net

D.A. Bunch Co.
4003 J Street SW, Ste. 4, Cedar Rapids, IA 52404
Craig Dostal: 319-399-1735; craig@dabunch.com

DPI Quality Custom Finishes
1402 18th St. SW, Cedar Rapids, IA 52404; Mark Debner
319-395-0225; mark.debner@dpifinishes.com

L & P Painting Company
PO Box 8325 900 3rd Ave. SW, Cedar Rapids, IA 52408
Scott Pinter: 319-362-8828; scottpinter@lppainting.com

Pospisil Painting, Inc. 728 58th Ave., Court SW
Cedar Rapids, IA 52404; Cliff Mohling: 319-362-4356
contactus@pospisilpainting.com

TBC Construction, LLC
PO Box 71031, Clive, IA 50325; Troy Berglund
515-491-5481; tberglund@windstream.net

Terry and Sons Painting, Inc.
3119 Lucas St., Muscatine, IA 52761; D. Terry
563-263-5866; dterry@terryandsons.com

KANSAS

Artisan Surface Systems
624 E. First Street, Wichita, KS 67202; Dustin Sorensen
316-264-0699; dustin@artisansurfacesystems.com

Cunningham Sandblasting & Painting Co., Inc. 5960 SE
Beasley Road, Riverton, KS 66770; James Brookshire
620-848-3030; sales@watertankspecialist.com

DEM Enterprises, LLC
PO Box 16115, Wichita, KS 67216; Hamza Smajlovic
316-351-1771; hamzasdem@yahoo.com

F & H Coatings, LLC
5003 E. 61st N. PO Box 250, Kechi, KS 67067
Sam Green 316-264-2208; samg@f-hcompanies.com

FH Companies
5003 E. 61st St. N., Kechi, KS 67067
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Performance Contracting, Inc. - Coatings Division 16047 W. 110th St., Lenexa, KS 66219; Chris Coffman 913-928-2808; chris.coffman@pcg.com	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•
PK Industrial 5351 SW 100th St., Augusta, KS 67010 Randal Hernandez: 316-775-6898; brian@pksti.com			•		•	•	•	•	•	•	•		•	•	•		
PROSOCO, Inc. 3741 Greenway Cir., Lawrence, KS 66046-5441 Stephen Falls: 785-830-7390; stephen.falls@prosoco.com			•								•						
Rob Carroll's Sandblasting and Painting, Inc. 12046 292nd Road PO Box 646, Arkansas City, KS 67005 Rob Carroll: 620-442-1361; rcsp@rcspblast.com								•	•	•	•	•		•	•		
Utility Maintenance Contractors, LLC 4151 N. Seneca St., Wichita, KS 67204 Kyle Long: 316-945-8833; kylel@umcllc.com		•	•		•		•	•	•	•	•			•	•		•

KENTUCKY

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Applied Science Technology, Inc. 118 Rochester Avenue, Louisville, KY 40214 Wayne Cundiff: 502-833-8556; waynec@astacme.com			•	•					•					•	•		
AQC 4310 Sunset Ave., Louisville, KY 40211 Aaron Kent: 502-386-8656	•									•		•			•		
AST Acme, Inc. 728 South 13th Street Louisville, KY 40210; Wayne Cundiff: 502-583-8355 wayne.cundiff@astacme.com			•	•	•	•			•	•				•	•		
BJB Restorations 958 Logan St., Louisville, KY 40204; Robert Plawinski 502-827-0330; robert@bjbrestoration.com											•	•				•	
Camaco, Inc. PO Box 675, Lebanon, KY 40033 Mitchell Cambron: 270-692-3174	•								•	•							
Earl True & Son Painting Contractors PO Box 2226, Ashland, KY 41105; David True 606-324-6510; earltruepaint@yahoo.com			•	•		•		•	•	•	•	•	•	•	•		
Fitzgerald Painting, Inc. 1250 Otter Pond Rd., Princeton, KY 42445 Melissa Fitzgerald: 270-365-2451	•	•	•				•										
Howell & Howell Contractors, Inc. 2603 Grassland Drive Louisville, KY 40299; Mike Dudukovich: 502-491-7985 md3@howellandhowellinc.com	•	•			•			•	•	•	•	•	•	•	•		
Huffman Tank & Tower, Inc. 210 Burksville Rd., Albany, KY 42602; Philip Huffman 606-387-6103; huffmantank@hotmail.com											•		•		•		
Intech Contracting, LLC 3131 Custer Dr. Suite 8, Lexington, KY 40517; Brad Wilder 859-272-0352; bradw@intechcontracting.com	•										•	•	•			•	
J & J Properties, Inc. 2631 River Rd., Hawesville, KY 42348; Jeff Gillette 270-929-0963; j.j.properties@hotmail.com	•	•	•	•	•			•	•	•	•	•	•	•	•		
McKinney Painting, Inc. 104 Kuhlman Dr., Versailles, KY 40383; Matthew Gamlin 859-983-4515; matt@mckinneypainting.com	•				•				•	•	•			•	•		
Peak Industrial Coatings & Linings PO Box 16560, Louisville, KY 40256; David Brooks 502-592-9169; david.brooks@peakcoatings.com	•	•			•			•	•	•	•	•			•		•
Pittsburg Tank & Tower Co. PO Box 913, Henderson, KY 42419; Don Johnston 270-826-9000; sales@watertank.com									•	•				•	•		
REM & Sons, Inc. 60 Waddill Ave. PO Box 770, Madisonville, KY 42431 Ed Moore: 270-821-3000; re.moore@att.net	•	•			•				•	•	•	•		•	•		
Rizzo Brothers Painting Contractors, Inc. 1564 Water St., Covington, KY 41014; Paul J. Rizzo 859-431-4700; paul@rizzobrothers.com			•						•	•		•		•	•		
West Ky Painting, LLC PO Box 2695, Paducah, KY 42002; Jerry Smallwood 270-554-2696; wkyp@bellsouth.net				•		•	•		•						•		

LOUISIANA

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Bilfinger Salamis 510 La Flamme Road, Broussard, LA 70518 Farrel LeBoeuf: 337-289-0092; fleboeuf@bissalamis.com			•	•	•		•	•				•	•		•	•	•
Brand Energy & Infrastructure Services 13527 Airline Hwy., Baton Rouge, LA 70817 Daniel Cavalier: 225-754-1104; daniel.cavalier@beis.com	•	•	•	•	•	•	•	•	•	•		•	•	•	•	•	•
Brock Services, LLC 200 Smith Street, West Monroe, LA 71292 Kenneth Harrell: 318-387-8778; kharrell@brockgroup.com		•		•	•	•	•					•		•	•		
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Commercial Coating Services Intl, LLC 3500 Old Spanish Trail E, New Iberia, LA 70560; Brian Krassic 936-539-3294; mikew@commercialcoating.com	•	•		•					•	•	•	•		•	•		
Crescent Coatings & Services PO Box 19346, New Orleans, LA 70119; Alvin Nelton 504-419-4549; abnelton@aol.com			•	•			•		•	•	•			•	•	•	•
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Empire Scaffold 9680 South Choctaw Drive, Baton Rouge, LA 70815 David Young: 225-924-4243; bcollins@empirescaffold.com			•	•	•	•	•		•	•			•				
Frisco Construction 128 West Woodlawn Ranch Rd., Houma, LA 70363 Chris Trahan: 985-688-3249; chrisfrisco@bellsouth.net	•		•	•				•						•		•	
Gulf Shipyard, LLC 2522 Main St., Baton Rouge, LA 70802 504-717-2896; gshipyard@aol.com			•								•	•	•	•	•	•	•
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Laiche & Co., Inc. PO Box 640519, Kenner, LA 70064; Whitley Laiche 504-469-2685; noemail@paintsquare.com	•	•	•		•	•	•	•	•	•	•				•		
Lanehart, Inc. 6530 Exchequer Dr., Baton Rouge, LA 70809 Gary Joyner; 225-756-5050; garypjoy@lanehart.net			•				•		•	•	•				•		
Marlin Services, LLC 50 Concord Road, Houma, LA 70360 Adam Martin; 985-346-0401			•	•	•								•	•	•	•	•
Petrin Corporation PO Box 330, Port Allen, LA 70767 225-343-0471			•	•		•	•	•	•	•		•	•	•	•		
Professional Application Services 15525 Knox Dr., Baton Rouge, LA 70817; Mark Alexander 225-756-4593; professappl@bellsouth.net										•	•						
Specialty Industrial, LLC PO Box 41270, Baton Rouge, LA 70835 Kelly Menge: 225-751-7334	•	•	•	•	•	•	•	•	•	•	•			•	•	•	
Stork Technical Services, Inc. 110 Matrix Loop, Lafayette, LA 70507; Ric Beard 337-261-9594; aric.beard@stork.com	•	•	•	•			•	•	•	•	•	•	•	•	•	•	•
Structural Protection Services, LLC PO Box 1222, 710 East Airline Hwy., Gramercy, LA 70052 Dale Abreo: 225-938-4105; spsllc@cox.net	•		•				•		•	•	•	•			•	•	
Sylvia T. Designs 2250 Relacher Dr., Mandeville, LA 70448 Sylvia Thompson: 985-778-1487; sylvia@sylviatdesigns.com														•			
Topcor Quality Applications 12025 Industriplex Blvd., Baton Rouge, LA 70809 Adam Saucier: 225-753-7067; jgary@topcor.com	•	•	•	•	•	•	•			•	•						
Vermillion Painting & Construction, Inc. PO Box 669, Benton, LA 71006; J. W. Kamphuis 318-469-8710; vermillion@aol.com			•			•	•	•	•	•	•			•	•	•	•
MAINE																	
Bath Iron Works 700 Washington St. MS 4610 Bath, ME 04530; Robert Cloutier: 207-442-2031; robert.cloutier@gdbiw.com			•									•			•	•	•
Central Maine Industrial Painting 39 Bugbee Rd., Dexter, ME 04930; Brian R. Parlin 207-924-6798; industrialpainting@yahoo.com		•	•				•	•	•	•				•	•		•
Copia Specialty Contractors, Inc. 235 Bomarc Road, Bangor, ME 04401; Lee Stanley 207-989-3082; copia@copiasc.com	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•

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Eagle Painting Company 111 Moose Hill Rd.,
Osborn, ME 04605; Christina Dawn Eagle: 207-469-5655
eaglepaintingcompany@gmail.com

Knowles Industrial Services Corp.
295 New Portland Rd., Gorham, ME 04038
Tim Rich: 207-854-1900; trich@knowlesindustrial.com

Marcel A. Payeur, Inc.
113 Otis Allen Rd., Sanford, ME 04073; Marcel A. Payeur
207-324-0786; cell@metrocast.net

Marston Industrial Services
109 Ridge Road,, Fairfield, ME 04937
Holly Marston: 207-514-4816; marston@gwi.net

Newport Industrial Fabrication, Inc.
445 Elm St., PO Box D, Newport, ME 04953; Tom Rumpf
207-368-4344; tom@nif-inc.com

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A I W, Inc.
5010 Inwood St., Hyattsville, MD 20781; Vahe Tanashian
301-277-8444; v-tanchian@americanironworks.com

Alpha Painting & Construction Co., Inc.
6800 Quad Ave., Baltimore, MD 21237; Betsy Aikaterinidis
410-288-6800; betsya@alphapainting.net

ASCM Artistic Stamped Concrete
5 Brit Oak Ct., Baltimore, MD 21234-1879; Bob or Beth
410-882-0921; stampedconcrete@comcast.net

Barrett Coatings
7521 Cemetery Lane Suite 201, Elkridge, MD 21075
Bruce Barrett: 410-796-8669; bruce@barrettcoatings.com

Bay Town Painting 137 Stahl Point Rd.
Baltimore, MD 21226; George Tarantino: 410-609-2626
baytownpainting@aol.com

Blastech Enterprises, Inc. 2200 Van Deman St. Suite 100
Baltimore, MD 21224; John Korfiatis: 410-633-7070
johnk@blastechenterprises.com

C&T Painting Co., Inc. 7026 Eastern Ave.
Baltimore, MD 21224; Christos Triantafillopoulos
410-282-8365; ctpainting@verizon.net

CMV Blasting, Inc.
10863 Cathell Rd., Berlin, MD 21811; Mauricio Soares
410-713-4706; sales@cmvblasting.com

Klicos Painting Company, Inc.
7728 Harford Rd., Baltimore, MD 21234
George Klicos: 410-668-1865

Manolis Painting Co., Inc.
539 North Dean St., Baltimore, MD 21205
Demetrios Manolis: 410-276-1369; d.manolis@verizon.net

Mediterranean Construction Co., Inc.
6903 N. Point Rd., Baltimore, MD 21219-1215
Margie Laskaridis: 410-477-2852; laskaridis1954@aol.com

O.T. Neighoff & Sons, Inc.
117 Holsum Way, Glen Burnie, MD 21060
Alan Hoyas, Sr.: 410-766-3000; otn@otneighoff.com

Prestige Floor Coating 2109 Emmorton Park Rd. Suite 209
Abingdon, MD 21040; Chris Adolph: 443-519-2628
sales@prestigefloorcoating.com

Reglas Painting Co., Inc.
4128 North Point Rd., Baltimore, MD 21222
Chris Reglas: 410-477-8024; tgreglas@aol.com

Shore Painting, Inc. Renovation & Restoration
204 15th St., Ocean City, MD 21842; Gail
410-289-4771; shorepntrestoration@comcast.net

Symmetric Painting, LLC
505 Blueball Rd. Bldg. 120F PO Box 701, Elkton, MD 21922
Laki Paliouras: 410-398-7717; laki@symmetricllc.com

Titan Industrial Services, Inc.
4054 North Point Rd., Baltimore, MD 21222
Pete Forakis: 104-771-8571; pete@titanindust.com

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214B South Meadow Rd., Plymouth, MA 02360
Peter Krukiel: 508-591-8400; peter@aandgis.com

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41 Sutton Ln., Worcester, MA 01603; Bob Peltier
508-479-2303; bob@prepcoat.com

AK Services, Inc.
347 Third St., Everett, MA 02149; Patrick Canonica
617-884-9252; pcanonica@akservices.com

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1 Maureen Ln., Georgetown, MA 01833
Tom Higgins: 978-376-8141; info@allsetcorp.com

Amex, Inc. Industrial Services
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Phil Baglione: 617-455-2466; sales@amexinc.net

The Aulson Company, LLC
49 Danton Drive, Suite 201, Methuen, MA 01844
Chris V. Pocoli: 978-609-7526; chrisp@aulsonllc.com

Black Bear Coatings & Concrete
644 River St., Fitchburg, MA 01420; Justin Tousignant
978-405-0017; info@blackbearconcrete.com

CAAS Maintenance
431 East St., South Hadley, MA 01075
Dan: 413-210-5930; caas@comcast.net

CJM Services, Inc. PO Box 424 50 Kerry Place
Norwood MA 02062; Charles J Morris: 781-440-0000
charlesmorris@cjmservicesinc.com

Corrpro Companies, Inc.
210 Winter Street, Suite 305, Weymouth, MA 02188
Charles Moran: 781-812-2689; cmoran@corrpro.com

Donlon Coatings, Inc.
175J New Boston St., Woburn, MA 01801
Sal Caraviello; 781-935-5111; salc@donloncoatings.com

The Duncan Group
69 Norman St., Everett, MA 02149; Howard Levine
617-389-8440; hlevine@duncangalvanizing.com

GAC Painting Co., Inc.
PO Box 929, Groton, MA 01450; Al Colburn
978-448-5723; gacpainting@gmail.com

Industrial Corrosion Services
10 Industrial Rd., Southwick, MA 01077
Justin Cigal: 413-569-5551; cigal@comcast.net

John W. Egan Co., Inc.
3 Border St., West Newton, MA 02465
Marc Cote: 617-244-6390; mcote@johnwegan.com

McDonnells Painting
PO Box 1475, Harwich, MA 02645; Peter McDonnell
508-430-1279; pmcdonnell@mcdonnellpainting.com

McLean Painting Co., LLC
3 Southside Rd., Unit 3, Danvers, MA 01923-3720
Ron: 978-774-4330; rwmclean@mcleanpainting.com

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New England Sandblasting and Painting PO Box 68, Rochester, MA 02770; Floyd Andrews 774-849-5892; nesandblast@aim.com	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•
Plastocor, Inc. 100 Research Rd., Hingham, MA 02043; Jim Mitchell 724-942-0582; jem@plastocor.com		•			•		•	•	•								
Prime Coatings, Inc. 161 Elm St., Salisbury, MA 01952; Richard Capolupo 978-465-2556; primerc@comcast.net	•		•			•			•	•	•	•	•				
Prusik Painting Company, Inc. 68 Central St., North Reading, MA 01864; Frank Prusik, Jr. 978-664-0897; frank@prusikpainting.com																	
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Tower Blast & Paint, Inc. PO Box 782, Middleton, MA 01949; Richard D. Tower, Jr. 978-774-5662; info@towerblastpaint.com			•		•	•			•	•		•					•
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Cox Brothers Machining, Inc. 2300 E. Ganson St., Jackson, MI 49202 Keith Cox; 517-796-4662; keith.cox@coxbro.com	•																
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George Kountoupes Painting Co. 661 Southfield Road Lincoln Park, MI 48146; Cary Kountoupes: 313-388-9400 carolyn@gkpainting.com										•	•	•			•		
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Pennington Brothers, Ltd. 5340 Holton Rd., Twin Lake, MI 49457; Ken Smith 231-828-4555; ksmith@penningtonbrothers.com			•	•		•		•		•	•				•		
Performance Floor Coating Systems, Inc. 165 N. Gratiot Ave., Mt. Clemens, MI 48043; Billy Campbell 586-468-3400; csj@performancefloor.com			•	•			•		•								
Ram Construction Services 13800 Eckles Road, Livonia, MI 48150 Kathy Gallina: 734-464-3800	•									•	•						•
Richard Brothers Painting 7730 Jackson Rd., Suite No. 4 Ann Arbor, MI 48103; Laura Bosko: 734-424-1406 staff@richardbrotherspainting.com											•			•			
Romer Decorative Solutions 9611 Roseland St., Livonia, MI 48150; Justin Romer 734-377-3323; justin@romerdecorative.com											•						
SaniCrete 24535 Hallwood Court, Farmington Hills, MI 48335 Keith Kwasny: 248-893-1000; info@sanicrete.com			•		•		•		•	•				•	•		
Seaway Painting, LLC 31801 Schoolcraft, Livonia, MI 48150; Niko Vlahakis 734-522-2440; niko@seawaypainting.com			•	•	•	•	•	•	•	•	•	•	•		•		

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Thermal-Tec/Midwest 13801 Francis Way, Cedar Springs, MI 49319 Linda Blanchard: 616-696-4000; lindab@thermal-tec.com		•	•				•		•								
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John Fransen: 320-983-6113
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Brainerd MN 56401; George H. Erickson: 320-692-2027
george@geospaint.com
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5858 152nd St. N., Hugo, MN 55038; Pete Karow
651-407-8000; pete@ipspaint.com
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Brian Gleeson: 651-755-7089; bgleeson@mmwaterjet.com
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Miesen Color Center
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507-723-4424; jam@newulmtel.net
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Nordic Tank & Tower, Inc.
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Mike Johnson: 218-564-5103
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763-535-4041; mdebuhr@rainbowincmn.com
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Mark Smith: 763-434-2470; mark@sbdecorating.com
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Superior Painting & Decorating, Inc.
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Rick Anderson; 763-786-8787; info@superior-ptg.com
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Swanson & Youngdale, Inc.
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952-545-2541; pskodje@swansonyoungdale.com
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TMI Coatings, Inc.
3291 Terminal Drive, St. Paul, MN 55121; Tracy Gliori
651-452-6100; tgliori@tmicoatings.com
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Tri-State Coatings
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United Painting & Sandblasting, Inc.
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218-729-7100; unitedpaintings@aol.com
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866-746-1445; claims@justlikenew-overspray.com
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36124 Highway 315, Batesville, MS 38606
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601-373-0374; Gmassey@metalcoaters.com

MS&G Painting, Inc.
1032 Raintree Dr. N., Pelahatchie, MS 39145
Mark Smitson: 601-854-7846; msmitson@yahoo.com
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9 County Road 370, Iuka, MS 38852; Steven Mills
662-423-5020; yellowcreekcoating@att.net
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314-991-3500; waltbazanjr@bazanpainting.com
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Brick City Painting, LLC
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573-473-7186; chad@brickcitypainting.com
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Tim Hughes: 636-947-4500; tim@ccistl.com
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800-628-4145; stevejr@coatingsunltd.com
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John Bubenik: 314-313-1495; john@colorcraftsmen.com
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144 S. Kentucky Ave., Independence, MO 64053
Curtis Goad: 800-733-4623; curtis@goadco.com
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William Stack: 314-862-8383; kochbros@sbcglobal.net
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Scott Slinkard: 573-335-9180; scott@lspainting.net
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Show Me Industrial, Inc. 2021 N. Warson Rd., St. Louis, MO 63114; Arturo Rosas 314-890-8030; showmeindustrial@sbcglobal.net							•				•	•					
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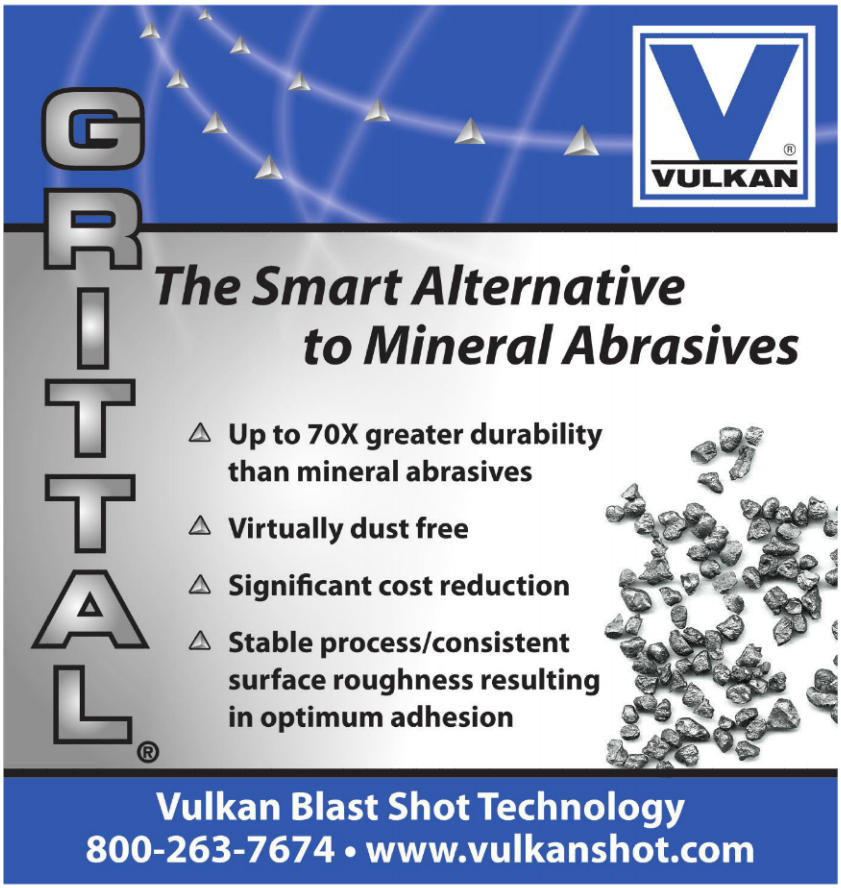

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Fletch's Sandblasting & Painting, Inc. 52 Shirking Rd., Epping, NH 03042; Bill Fletcher 603-679-3400; fletchssandblasting@hotmail.com	•		•						•		•	•	•	•	•		
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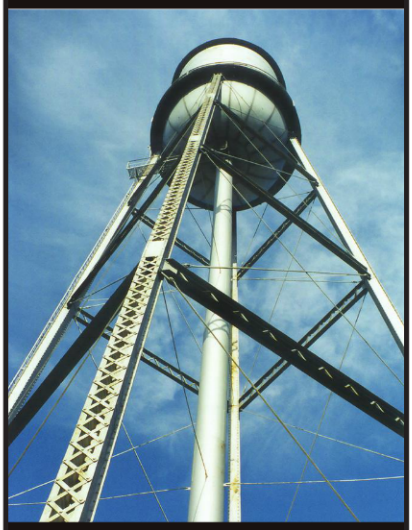

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516-305-5100; info@towermaint.com
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704-597-1166; dcptwork@bellsouth.net
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acullinan@floortechinc.com
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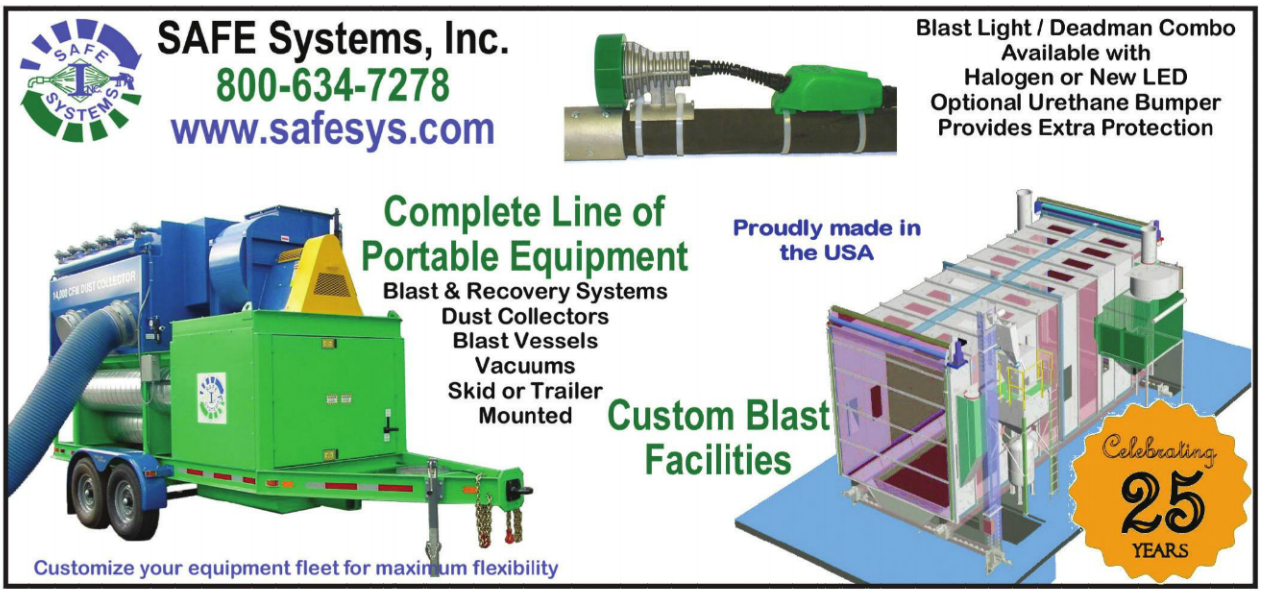

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PUERTO RICO																	
Demaco Corporation PO Box 8283, Ponce, PR 00732; Luigi Dessy 787-835-2222; ldessy@demacopr.com		•	•		•		•	•	•	•	•			•	•		
KF Painting Corp. PO Bon 6017 PMB 6027, Carolina, PR 00984 Ketlin Figueroa: 787-661-0820; kfpainting@hotmail.com		•	•	•	•		•		•	•	•	•		•	•	•	•
RYO Industrial Technologies, Inc. Box 896, Boqueron, PR 00622; Ines Oliver 787-254-3418; lnes@ryopr.com		•	•	•			•		•	•							
SmartCoatings PO Box 270047, San Juan, PR 00928 Néstor Jiménez 787-596-4040; coatings@spicogroup.com											•						
RHODE ISLAND																	
American Painting & Sandblasting, Inc. 57 Reservoir Rd., Coventry, RI 02816; Bob Crowe 401-826-1649; apsi45@verizon.net	•	•	•	•	•	•	•	•	•	•		•		•	•		
Century Drywall, Inc. 1988 Louisquisset Pike, Lincoln, RI 02865; Gary Ramsden 401-333-2140; info@centurydrywallinc.com	•	•	•	•	•	•	•	•	•	•							
Inter-City Contracting, Inc. 127 Limerock Rd., Smithfield, RI 02917; Kevin Bouthillette 401-231-5025; blasting@intercitycontracting.com	•	•	•		•			•	•	•				•	•	•	
J. Goodison Company, Inc. 244 Burlingham Ave., North Kingstown, RI 02852 Chris Braga: 401-667-5935; chris@jgoodison.com				•											•		•
L.F. Clavin & Company, Inc. 221 Hallene Road, Warwick, RI 02886 Bill Clavin; 401-737-3900; bill@lfclavin.com	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•

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SOUTH CAROLINA																	
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Dunlap, Inc. 110 Carrie Court, Greenville, SC 29615; Steve Dunlap 864-458-8401; sdunlap@dunlapic.com			•		•		•	•	•	•	•	•	•		•		•
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Exteriors International, LLC 710A Seaboard St., Myrtle Beach, SC 29577; Joe Guernsey 843-236-4400; info@exteriorsinternational.com												•					
Fisher Tank Company 104 Fisher Tank Drive, Leesville, SC 29070; Robert Colt 803-520-2750; rcolt@fishertank.com			•			•	•	•		•	•	•		•	•	•	•
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Rapid Deployment Systems 503 Belle Hall Parkway Ste. 102, Mount Pleasant, SC 29464; Geoff Hawes 843-552-0036; geoff@rapiddeployablesystems.com			•														
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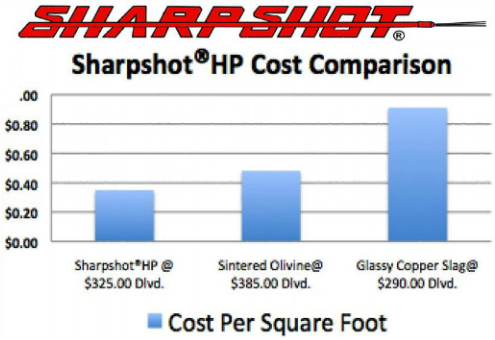

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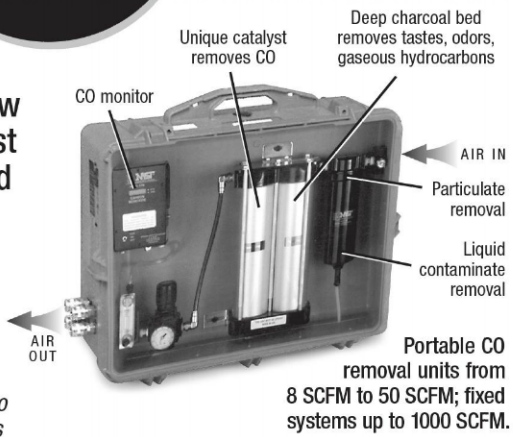

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304-623-3844; service@specialtygroups.com

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1081 Kanawha State Forest Drive, Charleston, WV 25314
Kenneth P. Bowen: 304-744-9431; wqw@frontier.com

WISCONSIN

Advanced Coatings Plus, LLC W4201 Black Point Rd.
Lake Geneva, WI 53147; Bill Grunow: 262-215-3508
advancedcoatingsplus@yahoo.com

Classic Protective Coatings, Inc.
N. 7670 State Hwy 25, Menomonie, WI 54751; Ray Witke
715-233-6267; heather@classicprotectivecoatings.com

DCS Tank Lining
1344 W. Franklin Street, Appleton, WI 54914
Tom Dean: 920-707-0622

Devine Co. of Milwaukee, Inc.
PO Box 33, Muskego, WI 53150-0033
Sean Devine: 262-679-1500; sdevine@wi.rr.com

Ed Weber Painting, Inc.
PO Box 106, Cedarburg, WI 53012
Ed Weber: 414-352-5440

	BRIDGE & HIGHWAY	CHEMICAL PROCESSING	MARINE & SHIPYARD	OFFSHORE OIL & GAS	POWER, CONVENTIONAL	POWER, NUCLEAR	REFINERY/PETROCHEM	TRANSMISSION PIPELINE	WASTEWATER TREATMENT	WATER TANKS/WORKS	CONCRETE COATING & REPAIR	LEAD PAINT REMOVAL	ACCESS & CONTAINMENT	SHOP PAINTING	TANK LININGS	THERMAL SPRAY	WATERJETTING
Executive Decorating, Inc. S75W21071 Field Dr., Muskego, WI 53150 Patrick Hade: 262-679-0875														•			
Finishing Plus N1009 Quality Dr., Greenville, WI 54942 Dean Dix: 920-915-3147; ddix@finishingpluswi.com	•		•		•			•		•				•			
Goldsmith Painting & Cleaning, Inc. 425 Forest Avenue Sheboygan Falls, WI 53085; Beth Goldsmith 920-467-4651; bgoldsmith@goldsmithpainting.com		•	•		•		•	•	•	•	•			•	•		•
Hess Sweitzer, Inc. 2805 S. 160th St., New Berlin, WI 53151; Tom Sweitzer 262-641-9100; tomsweitzer@hesssweitzer.com	•	•	•		•			•	•	•			•		•		
Howard Grote & Sons Painting, Inc. 4900 Ivywood Trail, McFarland, WI 53558; Kurt Grote 608-838-6756; krgrote@groteandsons.com		•		•		•	•	•	•	•	•	•	•	•	•		
Klein Dickert Company, Inc. 3166 Market St., Green Bay, WI 54304; Michelle Kocken 920-983-2334; northpaint@klein-dickert.com		•	•		•	•	•	•	•	•			•	•			
Klein-Dickert Co., Inc., Paint Division 1402 Emil St., Madison, WI 53713; Francis X. Dickert 608-258-3310; paint66@klein-dickert.com		•			•				•	•	•			•			
Lane Tank Company, Inc. PO Box 500 Menomonie, Mosinee, WI 54751; Matt Fisher 715-235-3110; lanetank@charter.net										•					•		
M and W Protective Coating Co., LLC 2239 16 3/4 Ave., Rice Lake, WI 54868; Doug Winkel 715-234-2251; noemail@paintsquare.com											•						
Midtek 229 W. Park Ave., Hartland, WI 53029 Frederick Ott: 262-367-3994; fredott3@aol.com		•	•		•	•	•	•			•				•		
Midwest Industrial Coatings-WI, LLC 1217 S. Grandview Blvd, Waukesha, WI 53188 Peter Bakke: 262-524-8110; peter@mic-llc.com		•	•	•		•	•	•	•	•	•		•		•		
Midwest Special Coatings PO Box 87, Caledonia, WI 53108; Dan Haley 262-639-8261; h-j@sbcglobal.net		•	•		•	•	•	•	•	•	•	•	•		•		
North Central Insulation 3204 Pleasant St., Altoona, WI 54720; Tom Hagen 715-835-4322; hagen@northcentralinsulation.com					•	•		•						•			•
Porta-Painting, Inc. 313 Travis Ln., Waukesha, WI 53189; Amanda Martin 262-970-9713; amanda.m@portapainting.com		•	•		•	•	•	•	•	•	•	•	•	•	•		
Precision Iceblast Corporation 801 Maple Street, Peshtigo, WI 54157; Keith Boye 708-205-1141; kboye@precision-iceblast.com	•	•	•	•	•	•	•	•	•	•	•	•	•		•		
Quality Sandblasting, LLC 1144 Ashwaubenon St. Green Bay, WI 54304; Sean Glanner: 920-337-2010 sean@qualitysandblasting-gb.com	•		•						•	•		•	•	•	•		
Recker Painting 1301 Rae Ln., Madison, WI 53711; Allen Recker 920-342-0158; reckerpainting@yahoo.com											•						
Reedy Concrete Construction, Inc. 16451 S. 15th St., Galesville, WI 54630; Prentice 608-582-4335; prentice@reedyconcrete.com											•						
Rockwell Painting, Inc. 1766 Drotning Rd. Stoughton, WI 53589; Vincent H. Rockwell 608-441-9350; rockwellptg@aim.com							•			•	•			•			
Rucinsky's Painting & Decorating, LLC 3680 N. Main Street, Oshkosh, WI 54901; Jon Rucinsky 920-236-9055; info@rucinskyspainting.com														•			
Safway Group Holding, LLC N19 W24200 Riverwood Dr., Waukesha, WI 53188 Jerry Dolly: 800-582-9391; info_request@safway.com	•	•	•	•	•	•	•	•	•	•	•	•	•		•		
Schneider & Schneider, Inc. 216 W. McWilliams St., Fond du Lac, WI 54935 Joe Schneider: 920-921-7575; schneidr@sbcglobal.net								•		•			•				
Service Painting Corp. 2727 W. Mill Road, Milwaukee, WI 53209; Will Stevens 414-247-9400; will@servicepaintingcorp.com	•	•	•		•		•	•	•	•	•	•	•		•		
Showcase Painting, LLC 3313 N 15th St., Sheboygan, WI 53083; Keith Barnstable 920-452-0901; kbarnstable@showcasepainting.net											•			•			
Spectrum Contracting Corporation 815 Beech Street, Grafton, WI 53024; Rob Stelter 262-377-5500; rstelter@spectrumcontracting.com	•	•	•		•	•	•	•	•	•			•	•	•		
Spiegel Sandblasting & Specialty Coatings, LLC N. 4125 Menzel Road, Tigerton, WI 54486 Everette Spiegel: 715-535-3179; spiegel_6@msn.com			•		•			•	•	•	•		•		•		
Spies Painting, Inc. N8003 Highway 151, Fond du Lac, WI 54937; Erik Spies 920-921-7107; erik@spiespainting.com				•	•		•	•	•	•	•	•	•		•		
Surf Prep, Inc. PO Box 470, Pewaukee, WI 53072; Adam Zelmer 262-746-1013; adamz@surfprep.com	•																•
Symet, Inc. 3282 N. 35th St., Milwaukee, WI 53216; Tim Ash 414-449-8770; info@symet.com	•	•			•		•		•	•				•	•		
Talatzko Painting, Inc. 4000 W. Rivers Edge Cir., Unit 10, Milwaukee, WI 53209 Mike Talatzko: 414-362-9770; talatzkopainting@aol.com											•						

WYOMING

	BRIDGE & HIGHWAY	CHEMICAL PROCESSING	MARINE & SHIPYARD	OFFSHORE OIL & GAS	POWER, CONVENTIONAL	POWER, NUCLEAR	REFINERY/PETROCHEM	TRANSMISSION PIPELINE	WASTEWATER TREATMENT	WATER TANKS/WORKS	CONCRETE COATING & REPAIR	LEAD PAINT REMOVAL	ACCESS & CONTAINMENT	SHOP PAINTING	TANK LININGS	THERMAL SPRAY	WATERJETTING
Ace Sandblasting & Coating, Inc. 3289 Prospector Dr. Unit #3, Casper, WY 82604 Travis Lawhorn: 307-237-8802	•		•				•	•	•	•	•			•	•		
AP, Inc. / Artectural Painting PO Box 12611, Jackson, WY 83002; Scott Berger 307-690-6884; scott@artectural.com											•	•		•			•
Carr Coatings PO Box 458, Lovell, WY 82431; Chad Carr 605-723-2677; chad@carrcoatings.com		•		•		•	•	•	•	•	•			•	•		
Mandros Painting, Inc. PO Box 1090, Green River, WY 82935; Bob Mandros 877-875-6555; office@mandros.net		•						•	•	•	•	•			•	•	
National Coatings, Inc. 9829 W Landmark Ln., Casper, WY 82604; Mike Jansens 208-941-5239; mpeters@nationalcoatings.biz		•	•		•	•	•	•	•	•	•			•	•		
S & L Industrial PO Box 126, Cowley, WY 82420; David Rael 307-548-2242; sli@tctwest.net		•						•	•	•	•	•	•		•		
Stiefco, LLC 4332 County Road 210, Burns, WY 82053 Mike Stief: 307-547-3471; stiefco@aol.com							•			•	•	•		•			

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